



## Zoning Administrator

### Regular Meeting **AGENDA**

**Civic Center Annex ♦ 300 Seminary Avenue., Conf. Rm. #3 ♦ Ukiah, CA 95482**

**Zoom Teleconference Link: <https://us06web.zoom.us/j/85026044542>**

**Call 669-444-9171 (US Toll-Free)**

**Meeting ID: 850 2604 4542**

**June 5, 2024 - 10:00 AM**

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1. **CALL TO ORDER**

2. **VERIFICATION OF NOTICE**

3. **APPEAL PROCESS**

All determinations of the Zoning Administrator regarding minor discretionary planning permits are final unless a written appeal, stating the reasons for the appeal, is filed with the City Clerk within ten (10) days of the date the decision was made. An interested party may appeal only if he or she appeared and stated his or her position during the hearing on the decision from which the appeal is taken. For items on this agenda, the appeal must be received by June 17, 2024.

4. **COMMENTS FROM AUDIENCE ON NON-AGENDA ITEMS**

The Zoning Administrator (ZA) welcomes input from the audience that is within the subject matter jurisdiction of the ZA. In order for everyone to be heard, please limit your comments to three (3) minutes per person and not more than ten (10) minutes per subject. The Brown Act regulations do not allow action to be taken on audience comments.

5. **APPROVAL OF MINUTES**

5.a. Approval of the March 14, 2024, Zoning Administrator Meeting Minutes

***Recommended Action: Staff Recommends Review & Approval of the March 14, 2024, Zoning Administrator Meeting Minutes***

Attachments:

1. ZAM\_20240314 - Draft

6. **SITE VISIT VERIFICATION**

7. **PUBLIC HEARING**

7.a. Request to approve a Minor Site Development Permit for the remodel of an existing gas-station by updating the exterior façade and facilitating interior tenant improvements, including conversion of the existing garage/automobile repair area to 'Retail Sales', various modifications and improvements to the parking lot, including new landscaping, trees, and the installation of revised signage at 734 South State Street; APN 003-031-78; File No. 24-9253

**Recommended Action: Staff recommends the Zoning Administrator 1) conduct a public hearing; 2) approve the minor site development permit (File No. 24-9253) based on the Findings and subject to the Conditions of Approval attached to the Staff Report.**

Attachments:

1. Draft Conditions of Approval
2. Draft Findings
3. Application Materials
4. Agency Referral Responses
5. DRB Minutes and Materials (04/25/24)

## **8. ADJOURNMENT**

Please be advised that the City needs to be notified 72 hours in advance of a meeting if any specific accommodations or interpreter services are needed in order for you to attend. The City complies with ADA requirements and will attempt to reasonably accommodate individuals with disabilities upon request. I hereby certify under penalty of perjury under the laws of the State of California that the foregoing agenda was posted on the bulletin board at the main entrance of the City of Ukiah City Hall, located at 300 Seminary Avenue, Ukiah, California, not less than 24 hours prior to the meeting set forth on this agenda.

Jesse Davis, Chief Planning Manager  
May 24, 2024

**CITY OF UKIAH**  
**ZONING ADMINISTRATOR MINUTES**  
**Meeting Held in Person and Via Teleconference**  
**March 14, 2024**  
**3:00 p.m.**

**STAFF PRESENT**

Craig Schlatter, Zoning Administrator  
Jesse Davis, Chief Planning Manager  
Joshua Montemayor, Contract Planner

**OTHERS PRESENT**

Hrant Ekmekjian

**1. CALL TO ORDER**

The meeting was called to order at 3:00 p.m.

*Zoning Administrator Craig Schlatter presiding.*

**2. VERIFICATION OF NOTICE**

**3. APPEAL PROCESS**

**4. COMMENTS FROM AUDIENCE ON NON-AGENDA ITEMS**

**5. APPROVAL OF MINUTES**

a) Approval of the October 26, 2023, Zoning Administrator - Meeting Minutes

**6. SITE VISIT VERIFICATION**

**7. PUBLIC HEARING**

**Request to approve a Minor Amendment (File No. 23-9153) to existing Major Use Permit (File No. 17-2841) to allow for on-site distribution of cannabis products at an existing cannabis retail business “Cannavine” located at 1230 Airport Park Boulevard, Suite C; APN 180-080-81**

Contract Planner, Joshua Montemayor, presented the Staff Report.

Zoning Administrator Craig Schlatter opened the public hearing at 3:15 p.m.

Speaker: Hrant Ekmekjian

Zoning Administrator Craig Schlatter closed the public hearing at 3:19 p.m.

Zoning Administrator Craig Schlatter approved the use permit modification request, subject to the Findings in Attachment 1 and the Conditions of Approval in Attachment 2. In addition, the Zoning Administrator requested that Staff clarify a minor typographical error in the Staff Report, as well further demarcate new or altered Conditions of Approval when finalized versions were provided to the Applicant and updated in the administrative record.

**8 ADJOURNMENT**

There being no further business, the meeting was adjourned at 3:21 p.m.

DRAFT



TO: Craig Schlatter, Zoning Administrator  
FROM: Joshua Montemayor, Contract Planner  
DATE: June 5, 2024  
SUBJECT: Request to approve a Minor Site Development Permit for the remodel of an existing gas-station by updating the exterior façade and facilitating interior tenant improvements, including conversion of the existing garage/automobile repair area to 'Retail Sales', various modifications and improvements to the parking lot, including new landscaping, trees, and the installation of revised signage at 734 South State Street; APN 003-031-78; File No. 24-9253.

**SUMMARY**

PROJECT NAME: File No. 24-9253 – Express Gas Station Remodel  
OWNER: BP Walia Inc  
498 Beltrami Drive  
Ukiah, CA 95482  
APPLICANT/AGENT: Boulevard Construction Inc.  
4080 Truxel Road  
Sacramento, CA 95834  
REQUEST: Minor Site Development Permit  
LOCATION: 734 S. State Street; APN 003-031-78  
TOTAL ACREAGE: ± 0.32 acre (13,916 sf)  
GENERAL PLAN: Community Commercial (CC)  
ZONING DISTRICT: Community Commercial (C-1)  
ENVIRONMENTAL DETERMINATION: Exempt, pursuant to CEQA Guidelines §15301, Class 1, Existing Facilities.  
RECOMMENDATION: Conditional Approval. Findings are included as Attachment 1 and Conditions of Approval are included as Attachment 2.

**BACKGROUND**

In April 1993, Use Permit Application 93-10 was approved by the City of Ukiah Planning Commission, allowing the continued operation of a full-service gas station with new underground fuel tank(s), as well as the implementation of a new sign program. The building



itself, constructed before 1961, previously housed Redwood Tree Service, which offered gas station and automobile repair services until the early-1990s. Service continued under various operators, including Exxon and 76, for the next few decades.

In 2019, the property underwent minor renovation through the approval of a Minor Use Permit (File No. 19-4525) for operation of a car detailing and window tinting business on the subject property, and within a section of the existing structure. However, the gas pumps themselves were not activated for the commercial use with that operator. Since 1993, however, the gas station pumps, and underground fuel tanks have remained operational and unchanged, as licensure has continued under various property owners, and required inspections conducted by the Mendocino County Environmental Health and Mendocino County Air Quality Management District (MCAQMD).

### **PROJECT DESCRIPTION**

The project aims to resume the commercial operations of the existing gas station while also undertaking refurbishments to the existing building and parking lot. Comprehensive design and construction details are provided by the applicant and can be reviewed in Attachment 3. Below, a summary list of the proposed improvements is provided:

- Remodel the exterior façade and extend upward the projection of the existing roof with a new parapet, resulting in an overall building height of  $\pm 18$  feet;
- Facilitate tenant improvements to the interior of the structure, including the conversion of the automobile repair area to allow for expanded 'Retail Sales' and accessory food sales. The existing footprint of the structure, however, will not be expanded;
- Resume operation of the existing fuel pumps and repaint the existing gas station and canopy under a unified color palette and brand;
- Update the existing monument sign, which will remain in its current location, and install new building signage featuring the business name "Express";
- Reduce the number of off-street parking spaces and facilitate improvements including new striping, upgraded lighting, and the installation of an EV charging station;
- Approximately 1,740 square feet, equivalent to 12.5% of the total site area, will be dedicated to new landscaping with irrigation. This will involve the creation of a new tree well within the rear parking lot area, the planting of six (6) additional trees, landscape planters with accent shrubs, and an evergreen hedge along the southern perimeter;
- Construct a new trash enclosure at the southwest corner of the property. Currently, no trash enclosure exists on site.
- As a component of this Minor Site Development Permit, the Owner/Applicant is subject to a Condition of Approval requiring compliance with all Mendocino County



Environmental Health, Mendocino County Agricultural Commissioner, and MCAQMD standards.

**RELATED APPLICATIONS ON-SITE**

- **1993:** Major Use Permit (#93-10)
  - Remodel an existing gas station to include new tanks, a sign program, building remodel (interior/exterior) and replacement of the existing canopy.
    - Prior to #93-10, no Discretionary Permit regulated the Subject Property.
- **2003:** Minor Subdivision (#03-35)
  - Minor Subdivision of the property into two (2) parcels; Parcel 1 – 13,600 square feet; Parcel 2 – 18,200 square feet
- **2019:** Minor Use Permit (#19-4525)
  - Utilization of structure to provide 'Car Detail' and 'Window Tint' services.
- **2022:** Business License: Cell-Phone Repair services

**LOCATION & EXISTING CONDITIONS**

The ±0.32-acre project site (APN 003-031-78) occupies the corner of South State Street and Freitas Avenue, accessible from both thoroughfares. Currently, the site accommodates a 1,680-square-foot commercial building, formerly utilized as a gas station, then auto detailing services, but most recently cell-phone repair. The site features existing gas pumps, as well as an underground fuel tank(s) with a capacity of 15,000 gallons.

Parking, consisting of 11 spaces with faded markings, which primarily is situated toward the rear of the property. Minimal landscaping is present, with only three (3) trees adorning the property's multiple frontages —two (2) along Freitas Avenue and one (1) along South State Street near the existing monument sign. A chain-link fence with vinyl slats delineates the southern boundary, separating it from an adjacent parking lot. Furthermore, the rear of the building opens toward another neighboring parking area to the west.

The site is zoned C1 (Community Commercial) and is surrounded by similarly designated C1 zoned parcels. Nearby commercial and retail establishments include the Ukiah Aikido Studio, a motel, and various restaurants. Large parking lots border the parcel on the southern and western sides. The site has access from two frontages: Freitas Avenue to the north and South State Street to the east.

Below, Table 1 provides a summary of surrounding land uses. Figure 1 provides a location map of the project. Figure 2 provides a zoning map. Figure 3 provides photos of the existing site.

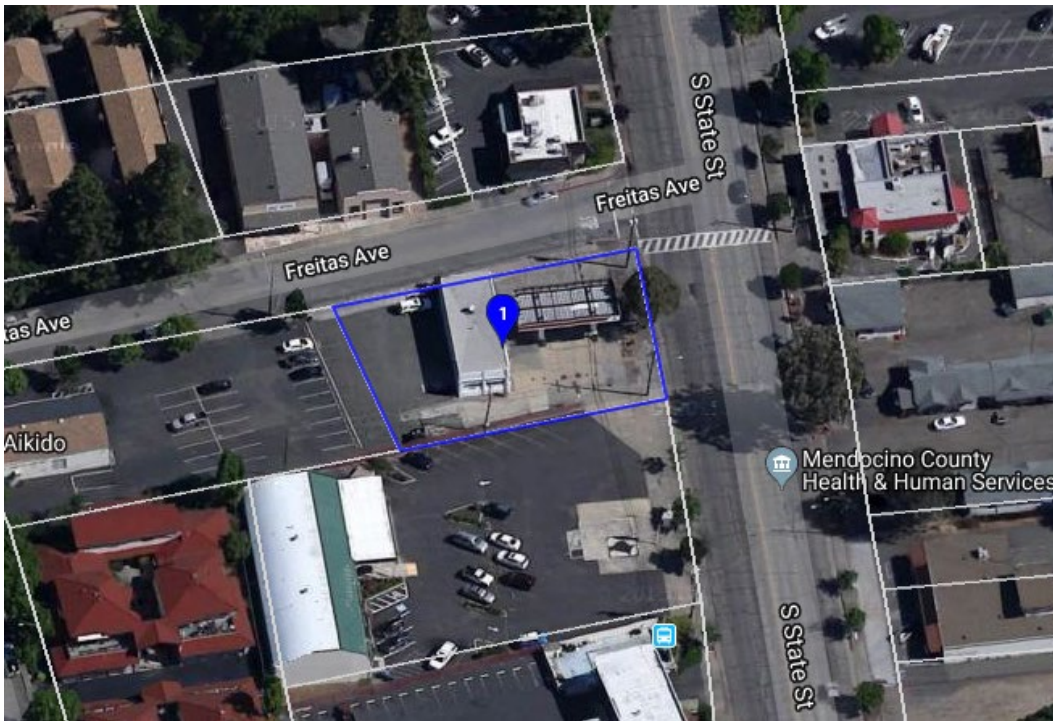
**TABLE 1 – SURROUNDING LAND USES**

	General Plan	Zoning	Uses
<b>North</b>	Commercial (C)	Community Commercial (C1)	Restaurant

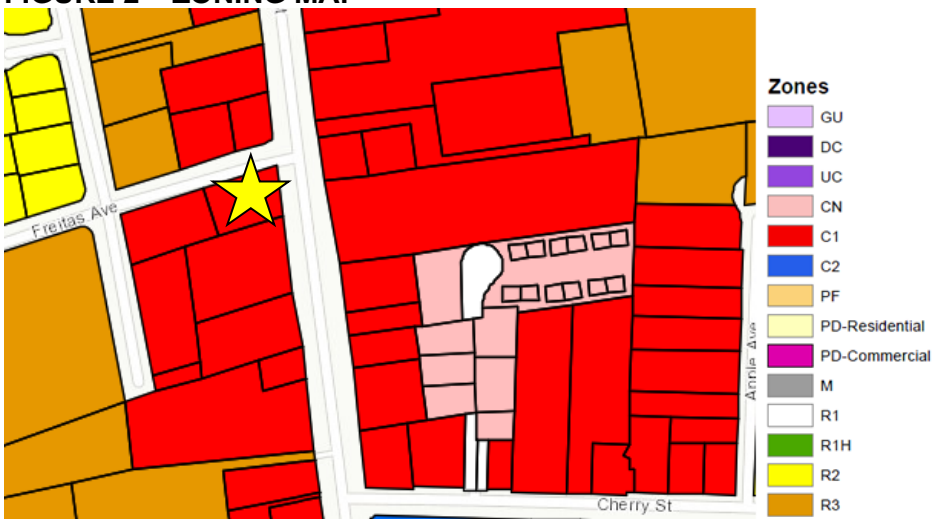


<b>East</b>	Commercial (C)	Community Commercial (C1)	Restaurant
<b>South</b>	Commercial (C)	Community Commercial (C1)	Parking Lot/Business Offices
<b>West</b>	Commercial (C)	Community Commercial (C1)	Parking Lot/Ukiah Aikido

**FIGURE 1 – LOCATION MAP**



**FIGURE 2 – ZONING MAP**





**FIGURE 3 – EXISTING SITE**

*View via South State Street*



*View via Freitas Avenue, front*



*View via Freitas Avenue, rear*



Notably, since these images were captured, the project site has deteriorated substantially. As



of May 23, 2024, the subject structure features boarded up window spaces, and an overall deterioration of site conditions.

### **STAFF ANALYSIS**

**GENERAL PLAN.** The General Plan land use designation for the parcel is Community Commercial (CC). This designation identifies areas generally located along major corridors in the city including State Street, with the intent of providing a transition between higher-intensity commercial uses along Highway 101 and residential neighborhoods on the western side of the city. This designation focuses on development for small and large format retail, shopping centers, chain restaurants, and personal service. The proposed Project would include redevelopment of a commercial property along the State Street Corridor. The final proposed Project incorporates significant input from Staff and received a positive recommendation from the City's Design Review Board. As reviewed, the request will aid in enhancing a prominent corner parcel, supporting the intent of the Community Commercial Designation. The Project supports growth and development of commercial and service uses in Ukiah to provide jobs and support the local economy. Lastly, the Project supports the following goals and policies contained within the Economic Development and Land Use elements of the General Plan. As such, Staff finds the Project consistent with the General Plan. Please refer to the Findings in Attachment 1 for more information.

#### Economic Development Element

- Goal ED-6: To maintain a supportive business climate and a healthy economy that leads to the expansion of existing businesses and the attraction of new ones.
- Goal ED-7: To grow the local economy and employment base by supporting efforts to retain, expand and attract local businesses.

#### Land Use Element

- Policy LU-4:1 High-Quality Building Design. The City shall encourage distinctive and high-quality commercial building design and site planning that respects the character of Ukiah.
- Policy LU-8.4: Reuse of Underutilized Property. The City shall encourage property owners to revitalize or redevelop abandoned, obsolete, or underutilized properties to accommodate growth.
- Goal LU-11: To ensure high-quality site planning, landscaping, and architectural design for all new construction, renovation, or remodeling.

**ZONING ORDINANCE.** The subject parcel is within the Community Commercial (C-1) zoning district. The purpose of the C-1 zoning district is to provide a broad range of commercial land use opportunities along primary transportation corridors within the city. It is intended to promote and provide flexibility for commercial development and encourage the establishment of community-wide commercial-serving land uses. The project site is already developed with the building, existing parking, driveway access, and street trees. Minor additions to the exterior of the building, upgrades to the parking lot, and landscape enhancements will improve the overall



appearance of the site and the project has been designed to be consistent with all applicable zoning regulations.

Auto service (gas) stations are allowed uses in the C-1 Zoning District with a Use Permit. The property retains an existing Use Permit from 1993 (MaUP No. 93-10) to allow the continued operation of the gas station at 734 South State Street. There are no proposed changes to the building footprint or any expansion to the gas pumps or canopy.

A summary of the relevant Design Requirements and compatibility with the proposed project is listed below.

<b>C-1 Zoning Design Requirements</b>	<b>Proposed Project</b>
Building Height – Max 50 ft.	Max 18 ft. to the top of the new parapet.
Building Site Area – Existing	No change.
Setbacks – Existing	No change.
Parking – Existing	<p>Refurbish parking lot, reduce parking spaces, repaint, and restripe and create EV charging infrastructure.</p> <p>10 standard parking stalls            1 ADA parking stall            4 fuel pump parking</p>
Signage	<p>Reface existing monument sign to match the color and aesthetic of commercial building and bear the business logo and name, "Express."</p> <p>The building façade is set to display the name "Express Mart" alongside the associated business logo.</p> <p>The fuel pump canopy will bear the name "Express" and its corresponding logo.</p> <p>All sign fonts and colors will match across each structure.</p>

**LANDSCAPING.** Under Ukiah Municipal Code (UMC) Sec. 9087(D), all development projects requiring discretionary review must include a landscaping plan that matches the size and scale of the proposed development. Although this is not a *new development*, the applicant has provided a Preliminary Landscape Plan (Attachment 3) to enhance the aesthetics of the existing parcel. The landscaping standards for this project are included as Conditions of Approval.

Currently, the site has minimal to negligible landscaping, with only a few existing street trees of



varying health remaining from the early-1990's. The Applicant has included landscaping features to improve the overall site aesthetics, such as perimeter planting strips, individual planting wells, and new shade trees. The total new landscape area will cover 1,740 square feet, or approximately 13% of the total site area.

**AIRPORT LAND USE CONSISTENCY:** Staff have reviewed the request and determined that the proposed project would not result in an expansion of the floor area or increase in the usage intensity (people per acre) beyond the limitations conveyed by the Ukiah Municipal Airport Land Use Comprehensive Plan (UKIALUCP) for the property operate as a gas station, and notes the project maintains a Major Use Permit from 1993 to operate a gas station facility.

While the project does request a minor increase in height to the structure, it is a design feature that remains substantially under the height limit of the C-1 zoning district, as well as adjacent structures and trees. Per Table 3A of the UKIALUCP, 'Gas Stations' are a conditionally compatible use within Zone 2, so long as the intensity criteria are met, and no airspace obstructions are facilitated. The UKIALUCP requires that gas stations feature fuel storage or 6,000 gallons or less. As noted earlier, the gas station presently maintains approximately 15,000 gallons of on-site underground fuel storage.

As the ALUC has no ability to reduce or remove nonconforming land uses from the airport environs, City of Ukiah Staff recommended that the ZA find the proposed project, a nonconforming gas station, as consistent and allow it to be refurbished as proposed in the application.

This determination is also based on the fact that the expanded gas station will feature fewer employees, due to the removal of automobile repair and services; maintain fewer parking spaces; and retains the same square footage of the existing structure.

**Design Review Board:** The project was presented to the Design Review Board (DRB) at their regular meeting on April 25, 2024. The DRB, which reviews construction, renovations, and alterations to building exteriors, serves as an advisory body to the Zoning Administrator on Minor Site Development Permits. During the meeting, the DRB evaluated the project's landscaping plan under Ukiah Municipal Code (UMC) Sec. 9087(D) and found the proposed landscaping features appropriate for the size and scale of the development.

Additionally, the DRB determined that the project complies with the Design Guidelines for Projects Outside of the Downtown Design District, specifically regarding site features, coordination, pedestrian access, signs, lighting, energy conservation, visual appearance, walkability, and universal design. The Board expressed support for the overall project and deemed the proposed renovations and alterations as appropriate. Consequently, there were no design recommendations, and the DRB recommended approval of project to the Zoning Administrator, although members did express support for the incorporation of alternative energy systems, such as solar panels, and electric vehicle charging stations. Design Review Board Minutes and Materials are included as Attachment 5.

## **AGENCY COMMENTS**



Project referrals were sent to the following responsible or trustee agencies with jurisdiction over the Project: City Building Official, Public Works, Ukiah Valley Fire, Police, Electric Utility, Mendocino County Environmental Health, and PG&E.

Responses were received from Electric Utility, Ukiah Police and PG&E. No other agencies provided response or comments. As required, the comments from the responsive agencies are included in the recommended conditions of approval. The full responses are provided in Attachment 4. Regardless of the limited comments received, prior to the resumption of operations, the project is conditioned to ensure that all appropriate permits are updated and provided, as required by local, regional or State entities.

### **MINOR SITE DEVELOPMENT PERMIT**

Projects exempt from the site development permit process include interior building remodels, repair and maintenance of structures or parking areas, and minor alterations on building exteriors. As this project proposes to refurbish an existing structure and subsequent site improvements to the parking area and accessory structures, it is considered a minor new development within the discretionary review of the Zoning Administrator. This aligns with a precedent established for a similar project (File No. 18-3605) at 390 East Gobbi Street (APN 002-312-11 reviewed by the Zoning Administrator on June 27, 2019.

In order to approve a Site Development Permit, the findings included in UCC §9263(E) are required to be made. The required findings and staff's analysis are included in Attachment 1. Staff finds the project to be consistent with the required findings for approval of a Site Development Permit.

### **ENVIRONMENTAL REVIEW**

The proposed project is exempt from the provisions of the California Environmental Quality Act (CEQA) pursuant to §15301 Class 1, Existing Facilities, which consists of the operation, repair, maintenance, leasing, licensing, or minor alteration of existing public or private structures. The key consideration is whether the project involves negligible or no expansion of an existing use. Based on the following, the proposed project is eligible to use this exemption.

- The site is developed with an existing building and parking lot, and utilities and services are already available at the site.
- The proposed renewal would continue operations of the existing dispensary; No expansion of the existing building footprint is proposed; and
- The location is not environmentally sensitive and does not include any drainage courses or bodies of water (such as creeks or streams).

### **PUBLIC NOTICE**

A notice of public hearing was provided in the following manner, as required by UCC §9262(C):

- Posted in two places on the project site on May 24, 2024;



- Mailed to property owners within 300 feet of the project site on May 24, 2024;
- Published in the Ukiah Daily Journal on May 24, 2024; and
- Posted at the Civic Center (glass case) 72 hours prior to the public hearing.

As of the writing of this staff report no correspondence has been received.

### **RECOMMENDATION**

Staff recommends the Zoning Administrator 1) conduct a public hearing; 2) approve the minor site development permit (File No. 24-9253) based on the Findings and subject to the Conditions of Approval attached to the Staff Report.

### **ATTACHMENTS**

1. Draft Findings
2. Draft Conditions of Approval
3. Application Materials
4. Agency Referral Responses
5. Design Review Board Minutes & Materials (4/25/24)

**CONDITIONS OF APPROVAL  
MINOR SITE DEVELOPMENT PERMIT FOR GAS STATION REMODEL  
AT 734 S. STATE STREET; APN 003-031-78  
FILE NO: 24-9253**

The following conditions of approval are supported by and based on information contained within the Zoning Administrator staff report dated June 5, 2024, the application materials and documentation, and the public record, in accordance with UCC Section 9263 (Site Development Permit Procedures). In the event of any conflicts with prior conditions, the conditions specified for this project take precedence. The Minor Site Development Permit is subject to the following Conditions of Approval.

Standard - Conditions of Approval

1. This approval is not effective until the ten (10) day appeal period applicable to this Planning permit has expired without the filing of a timely appeal. If a timely appeal is filed, the project is subject to the outcome of the appeal and shall be revised as necessary to comply with any modifications, conditions, or requirements that were imposed as part of the appeal.
2. All use, construction and the location thereof, or occupancy, shall conform to the application and to any supporting documents submitted therewith, including any maps, sketches, or plot plans accompanying the application or submitted by applicant in support thereof.
3. Any construction shall comply with the "Standard Specifications" for such type of construction now existing or which may hereafter be promulgated by the Engineering Department of the City of Ukiah; except where higher standards are imposed by law, rule, or regulation or by action of the Zoning Administrator such standards shall be met.
4. Building permits shall be issued within two (2) years after the effective date of the Planning Permit or same shall be null and void.
5. In addition to any condition which might be imposed; any construction shall comply with all building, fire, electric, plumbing, occupancy, and structural laws, rules, regulations, and ordinances in effect at the time the Building Permit is approved and issued.
6. The Applicant shall obtain all required Sign Permits, in compliance with Division 3, Chapter 7, Signs, of the UCC. Prior to the placement/installation of any sign(s), the applicant shall make application for and receive approval of a sign permit. Any signage shall be in substantial conformance with the Airport Industrial Park Planned Development Ordinance design and development standards.
7. A Final Landscaping Plan shall be submitted prior to issuance of Building Permit, in accordance with all applicable City codes. All landscaping shall be irrigated and maintained to a satisfactory condition throughout the life of the project.

8. The Applicant shall submit verification of all applicable permits or approvals in compliance with all local, state and federal laws to the Community Development Department prior to issuance of building permits.
9. All fees associated with the project planning permits and approvals shall be paid in full prior to occupancy.
10. As outlined in Article 20, Administration and Procedures, of the Zoning Code this planning permit may be revoked through the City's revocation process if the approved project related to this Permit is not being conducted in compliance with these stipulations and conditions of approval; or if the project is not established within two years of the effective date of this approval; or if the established use for which the permit was granted has ceased or has been suspended for 24 consecutive months.

#### From the Planning Division

11. The property and/or business owner shall obtain a valid permit from the Mendocino County Environmental Health Department to allow the operation of the site and use of the underground storage fuel tank. This permit must be obtained, and a valid copy must be filed with the city prior to the issuance of a final certificate of occupancy.
12. Application for and approval of a Sign Permit from the Community Development Department is required prior to installation of any signage.
13. The Applicant shall submit a final Landscaping and Irrigation Plan pursuant to Ukiah City Code Section 9087(D) for approval prior to Building Permit issuance.
14. The property and/or business owner shall obtain and maintain all required licensure to operate a gas station facility from local, regional, and State agencies, including but not limited to the City of Ukiah, Mendocino County Environmental Health Division, Mendocino County Agricultural Commissioner, and the Mendocino County Air Quality Management District.
15. As outlined in Article 20, Administrative and Procedures, of the Zoning Code this planning permit may be revoked through the City's revocation process if the approved project related to this Permit is not being conducted in compliance with these stipulations and conditions of approval; or if the project is not established within two years of the effective date of this approval; or if the established use for which the permit was granted has ceased or has been suspended for 24 consecutive months.

#### Building Inspection - Standard

16. A building permit is required for any interior or exterior modifications to the building deemed necessary for operation of the facility. Applicant shall submit plans, a building permit application, and a hardship form. Applicant shall submit four complete plan sets, two wet stamped and signed.

17. The design and construction of all site alterations shall comply with the 2022 California Building Code, 2022 Plumbing Code, 2022 Electrical Code, 2022 California Mechanical Code, 2022 California Fire Code, 2022 California Energy Code, 2022 Title 24 California Energy Efficiency Standards, 2022 California Green Building Standards Code, City of Ukiah, and Ukiah Valley Fire Authority Ordinances and Amendments.

#### Electric Utility- Standard

18. If the project includes solar, the size of the system may require a solar study before energizing. Interconnection shall not occur until study has been completed. There will be a fee for the solar study that is determined by the size of the proposed system. Please Contact Sonu Upadhyay at 707-463-6294.
19. The developer is to provide EUSERC approved electrical equipment that is to be used on this project. The COU EUD EUSERC acceptability chart shall be provided to customer. EUSERC does not allow source and load power (Metered and Un-Metered) to come in the same space.
20. All future site improvements shall be submitted to the Electric Utility Department for review and comment. At this time, specific service requirements, service Voltage and developer costs and requirements will be determined.
21. Developer shall provide detailed projected load calculations to the EUD which include one-line diagram.
22. Developer/customer shall incur all costs of this future project to include (labor, materials, equipment, engineering, and future services, and meter fees).
23. There shall be an easement provided to the EUD for any electric service that transverses through the property or around any City owned Electric equipment.
24. Customer shall follow the COU specifications outlined in the General Construction packet.

**FINDINGS**  
**MINOR SITE DEVELOPMENT PERMIT FOR GAS STATION REMODEL**  
**AT 734 S. STATE STREET; APN 003-031-78**  
**FILE NO: 24-9253**

The following findings are supported by and based on information contained within the Zoning Administrator staff report dated June 5, 2024, the application materials and documentation, and the public record, in accordance with UCC Section 9263 (Site Development Permit Procedures).

1. *The proposal is consistent with the goals, objectives, and policies of the City General Plan.*

The parcel's General Plan land use designation is Community Commercial. This designation typically applies to areas along major city corridors, such as State Street, and aims to transition between high-intensity commercial uses along Highway 101 and residential neighborhoods on the city's west side. It focuses on development that serves both the community and external consumers. Reactivating the existing gas station and associated commercial building aligns with the General Plan by providing a valuable resource for residents and visitors. Lastly, the Project supports the following goals and policies contained within the Economic Development and Land Use elements of the General Plan. As such, Staff finds the Project consistent with the General Plan.

Economic Development Element

- Goal ED-6: To maintain a supportive business climate and a healthy economy that leads to the expansion of existing businesses and the attraction of new ones.
- Goal ED-7: To grow the local economy and employment base by supporting efforts to retain, expand and attract local businesses.

Land Use Element

- Policy LU-4:1 High-Quality Building Design. The City shall encourage distinctive and high-quality commercial building design and site planning that respects the character of Ukiah.
- Policy LU-8.4: Reuse of Underutilized Property. The City shall encourage property owners to revitalize or redevelop abandoned, obsolete, or underutilized properties to accommodate growth.
- Goal LU-11: To ensure high-quality site planning, landscaping, and architectural design for all new construction, renovation, or remodeling.

2. *The location, size, and intensity of the proposed project will not create a hazardous or inconvenient vehicular or pedestrian traffic pattern.*

The location, size, and intensity of the proposed project will not create hazardous or inconvenient vehicular or pedestrian traffic patterns. Since the site already exists and most conditions will remain unchanged, the commercial building and fuel pumps will stay in place without expansion. The proposed improvements, including restriping the parking lot, incorporating designated paths of travel, enhancing lighting, and adding landscaping, will improve the overall vehicular and pedestrian traffic flow and safety.

3. *The accessibility of off-street parking areas and the relation of parking areas with respect to traffic on adjacent streets will not create a hazardous or inconvenient condition to adjacent or surrounding uses.*

Access to off-street parking areas via existing driveway aprons on Freitas Avenue and South State Street will be maintained, ensuring that vehicular and pedestrian traffic patterns remain safe and convenient for adjacent and surrounding uses.

4. *Sufficient landscaped areas have been reserved for purposes of separating or screening the proposed structure(s) from the street and adjoining building sites and breaking up and screening large expanses of paved areas.*

Sufficient landscaped areas have been reserved to separate or screen existing structures from the street and to break up large expanses of paved areas. Although this is not a new development, the project will enhance the site's aesthetics with the incorporation of new landscaping. Currently, the site lacks landscaping except for a few existing street trees. The new plan includes planting strips, individual planting wells, perimeter planting, and new shade trees, totaling 1,740 square feet or approximately 13% of the site area. The landscaping standards for this project are included as conditions of approval.

5. *The proposed development will not restrict or cut out light and air on the property, or on the property in the neighborhood; nor will it hinder the development or use of buildings in the neighborhood or impair the value thereof.*

The proposed project will not restrict light and air on the property or in the neighborhood, nor will it hinder the development or use of neighboring buildings or impair their value. The site is adjacent to parking lots to the west and south, with no nearby buildings that would be impacted. The development involves remodeling the existing building façade without significantly increasing the building's height or expanding its mass.

6. *The improvement of any commercial or industrial structure will not have a substantial detrimental impact on the character or value of an adjacent residential zoning district.*

The parcels immediately surrounding the project site are zoned Commercial (C-1), and there are no adjacent residential zoning districts.

7. *The proposed development will not excessively damage or destroy natural features, including trees, shrubs, creeks, and the natural grade of the site.*

There are no existing natural features on the project site, except for street trees along Freitas Avenue and South State Street. The project has been conditioned to preserve these trees, as well as install and maintain landscaping.

8. *There is sufficient variety, creativity, and articulation to the architecture and design of the structure(s) and grounds to avoid monotony and/or a box-like uninteresting external appearance.*

The project was reviewed and recommended for approval by the Design Review Board. The project has been designed to include variety, creativity, and articulation to the architecture and

design of the structure(s), including new façade features, therefore avoiding the monotony and/or a box-like uninteresting external appearance.

DRAFT

**ABBREVIATIONS:**

AB. ANCHOR BOLT	JAN. JANITOR
AB. AGGREGATE BASE	JT. JOINT
ABV. ABOVE	LAM. LAMINATE
ACoust. ACOUSTICAL	LAV. LAVATORY
AD. AREA DRAIN	LBS. POUNDS
ADJ. ADJUSTABLE	LVR. LOUVER
AHJ. AUTHORITY HAVING JURISDICTION	MACH. MACHINE
ALUM. ALUMINUM	MATL. MATERIAL
ALT. ALTERNATE	MAX. MAXIMUM
APPROX. APPROXIMATE	M.B. MACHINE BOLT
ARCH. ARCHITECT(URAL)	M.C. MEDICINE CABINET
BITUM. BITUMINOUS	MECH. MECHANICAL
BLDG. BUILDING	MFR. MANUFACTURER
BLK. BLOCK	MIN. MINIMUM
BLKG. BLOCKING	MIR. MIRROR
BM. BEAM	MISC. MISCELLANEOUS
BTM. BOTTOM	M.O. MASONRY OPENING
BD. BOARD	MTD. MOUNTED
BUR. BUILT-UP ROOF	MTL. METAL
CAB. CABINET	(N) NEW
CB. CATCH BASIN	N. NORTH
CEM. CEMENT	N.J.C. NOT IN CONTRACT
CER. CERAMIC	NO. OR # NUMBER
CFCI. CONTRACTOR FURNISHED CONTRACTOR INSTALLED	NOM. NOMINAL
CFOI. CONTRACTOR FURNISHED OWNER INSTALLED	N.T.S. NOT TO SCALE
CJ. CONSTRUCTION (CONTROL) JOINT	O.C. ON CENTER
CL. CENTER LINE	O.D. OUTSIDE DIAMETER (DIM.)
CLG. CEILING	OFF. OFFICE
CLT. CLEAR	O.F. OWNER FURNISHED
CMU. CONCRETE MASONRY UNIT	O.I. OWNER INSTALLED
COL. COLUMN	OPNG. OPENING
CONC. CONCRETE	OPP. OPPOSITE
CONN. CONNECTION	P.D.F. POWDER DRIVEN FASTENER
CONST. CONSTRUCTION	PERF. PERFORATED
CONT. CONTINUOUS	PNL. PANEL
CORR. CORRIDOR	PL. PLATE
CSK. COUNTERSUNK	PLAS.LAM. PLASTIC LAMINATE
CNTR. COUNTER	PLYWD. PLYWOOD
CTR. CENTER	PT. POINT
DBL. DOUBLE	P.T.D. PAPER TOWEL DISPENSER
DEPT. DEPARTMENT	P.T.D./R COMBINATION PAPER TOWEL DISPENSER & RECEPTACLE
D.F. DRINKING FOUNTAIN	RECEPTACLE
DTL. DETAIL	PART. PARTITION
DIA. DIAMETER	P.T.R. PAPER TOWEL RECEPTACLE
DIAG. DIAGONAL	Q.T. QUARRY TILE
DEM. DIMENSION	R. RISER/RADIUS
DISP DISPENSER	R.D. ROOF DRAIN
DN. DOWN	REF. REFERENCE
DWR. DRAWER	REFR. REFRIGERATOR
DS. DOWNSPOUT	REINF. REINFORCE(D).
DEG. DRAWING	(ING)
EA. EACH	REQD. REQUIRED
EJ. EXPANSION JOINT	REQMS. REQUIREMENTS
EL. ELEVATION	RESIL. RESILIENT
ELEC. ELECTRICAL	R.O. ROUGH OPENING
ELEV. ELEVATOR	RWD. REDWOOD
ENCL ENCLOSURE	R.W.L. RAIN WATER LEADER
ELEC. PNL. ELECTRICAL PANEL	S.C. SOLED CORE
EQ. EQUAL	S.C.D. DEAR COVER
EQUIP. EQUIPMENT	DISPENSER
E.W.C. ELECTRIC WATER COOLER	SCHED. SCHEDULE
EXIST. (E) EXISTING	S.A. SOAP DISPENSER
EXP. EXPANSION	SECT. SECTION
EXT. EXTERIOR	SH. SHELF
F.A. FIRE ALARM	SHWR. SHOWER
F.D. FLOOR DRAIN	SHT. SHEET
FDN. FOUNDATION	SIM. SIMILAR
F.E. FIRE EXTINGUISHER	S.N.D. SANITARY NAPKIN DISPENSER
F.E.C. FIRE EXTINGUISHER CABINET	S.N.R. SANITARY NAPKIN RECEPTACLE
F.F. FINISH FLOOR	SPEC. SPECIFICATION
F.H.C. FIRE HOSE CABINET	SQ. SQUARE
F.H.W.S. FLAT HEAD WOOD SCREW	ST. STL. STAINLESS STEEL
FIN. FINISH	STL. STEEL
FLASH. FLASHING	STOR. STORAGE
FLR. FLOOR	STRUCT. OR STR. STRUCTURAL
FLOUR. FLUORESCENT	SUSP. SUSPEND(ED)
F.O.C. FACE OF CONCRETE	T. TREAD
F.O.F. FACE OF FINISH	T.D. TRENCH DRAIN
F.O.S. FACE OF STUD	TEL. TELEPHONE
FR. FRAME	TEMP. TEMPERED
F.T. FIRE-RETARDANT TREATED	T. & G. TONGUE AND GROOVE
FT. FOOT OR FEET	THK. THICK(NESS)
FURR. FURRING	T.O.B. TOP OF BEAM
GA. GAUGE	T.O.C. TOP OF CURB
GALV. GALVANIZED	T.O.S. TOP OF SLAB
G.I. GALVANIZED IRON	T.O.P. TOP OF PAVEMENT
GL. GLASS	T.O.W. TOP OF WALL
GND. GROUND	T.P.D. TOILET PAPER DISPENSER
GWB. GYPSUM WALLBOARD	TRANS. TRANSFORMER
GYP. GYPSUM	T.S.B. TOP SET BASE
GYP. BD. GYPSUM BOARD	T.V. TELEVISION
H.B. HOSE BIBE	TYP. TYPICAL
H.C. HOLLOW CORE	U.N.O. UNLESS NOTED OTHERWISE
HDWD. HARDWOOD	UR. URINAL
H.M. HOLLOW METAL	U/S UNDERSIDE
HORIZ. HORIZONTAL	V.C.T. VINYL COMPOSITION TILE
HR. HOUR	VERT. VERTICAL
HT. HEIGHT	VEST. VESTIBULE
I.D. INSIDE DIAMETER (DIM.)	W/ WITH
INSUL. INSULATION	W.C. WATER CLOSET
INT. INTERIOR	WD. WOOD

**GENERAL NOTES:**

**CONTRACTOR RESPONSIBILITIES**

1. THE CONTRACTOR SHALL EXAMINE THE DRAWINGS AND SPECIFICATIONS AND SHALL NOTIFY THE ARCHITECT OF ANY DISCREPANCIES HE MAY FIND BEFORE PROCEEDING WITH THE WORK.
2. THE CONTRACTOR SHALL VERIFY CONDITIONS AT THE SITE AND REPORT ANY DISCREPANCIES TO THE ARCHITECT BEFORE PROCEEDING WITH THE WORK.
3. THE CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT PRIOR TO ANY EXCAVATING. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING UTILITIES BELOW GRADE AND RELATED SERVICE CONNECTIONS WITH UNDERGROUND SERVICE ALERT (USA), UTILITY PROVIDERS AND COUNTY.
4. THE CONTRACTOR SHALL COORDINATE REMOVAL, ABANDONMENT AND/OR RELOCATION OF EXISTING UTILITIES ABOVE OR BELOW GRAND WITH THE RESPECTIVE UTILITY PROVIDER AND FACILITY OWNER.
5. THE CONTRACTOR SHALL PERFORM ALL WORK WITHIN STREET TIGHT-OF-WAYS ACCORDING TO THE APPROVED CITY STANDARD PLANS AND SPECIFICATIONS.
6. THE CONTRACTOR SHALL PROVIDE TEMPORARY BRACES, SHORES, AND GUYS REQUIRED TO SUPPORT ALL LOADS TO WHICH THE BUILDING STRUCTURES AND COMPONENTS, ADJACENT SOILS, AND STRUCTURES, UTILITIES, AND RIGHT-OF-WAYS MAY BE SUBJECT TO DURING CONSTRUCTION.
7. FLOOR AND WALL OPENINGS, SLEEVES, VARIATIONS IN THE STRUCTURAL SLAB ELEVATIONS, DEPRESSED AREAS, AND ALL OTHER ARCHITECTURAL, MECHANICAL, CONTRACTOR PROCEEDS WITH CONSTRUCTION.

**CONSTRUCTION DOCUMENTS**

1. ALL DRAWINGS, ISSUED SEPARATELY AS CONSTRUCTION PACKAGES INCLUDING ALL DETAILS, SPECIFICATIONS, AND SCHEDULES, BOUND SEPARATELY, ARE PART OF THE CONTRACT DRAWINGS.
2. ITEMS MARKED "N.I.C." ARE NOT IN CONTRACT. SUCH ITEMS ARE INCLUDING IN THE DOCUMENTS WHEN CONTRACTORS COORDINATION FOR CONSTRUCTION IS REQUIRED.
3. DIMENSIONS: A) IN NO CASE SHALL WORKING DIMENSIONS BE SCALED FROM PLANS, SECTIONS, OR DETAILS ON DRAWING. B) ALL DIMENSIONS TO OPENINGS ARE TO THE FINISHED FACE UNLESS NOTED OTHERWISE. C) ALL DIMENSIONS TO STUD PARTITIONS ARE TO THE FACE OF FINISH UNLESS NOTED F.O.S. (FACE OF STUD). D) CEILING HEIGHT DIMENSIONS ARE FROM FINISH FLOOR TO FINISH FACE OF CEILING. E) ALL DIMENSIONS SHALL BE VERIFIED IN THE FIELD BEFORE PROCEEDING WITH THE WORK.
4. DETAILS MARKED "TYPICAL" SHALL APPLY IN ALL CASES UNLESS SPECIFICALLY NOTED OTHERWISE.
5. WHERE NO SPECIFIC DETAIL IS SHOWN, THE FRAMING, OR CONSTRUCTION SHALL BE IDENTICAL OR SIMILAR TO THAT INDICATED FOR LIKE CASES OF CONSTRUCTION ON THE PROJECT. PROVIDE AND INSTALL U.L. APPROVED FIRE STOPPING AND WRAPS AT ALL PENETRATIONS PER CHAPTER 7 OF THE CALIFORNIA BUILDING CODE AND THE UNIFORM FIRE CODE. SEE DRAWINGS FOR TYPICAL DETAILS.
6. DOOR SIZES INDICATED ON DOOR SCHEDULE ARE DOOR DIMENSIONS. ALLOWANCES FOR THRUSH HOLDS SHALL BE TAKEN OFF DOOR HEIGHT.
7. REGARDLESS OF OCCUPANT LOAD, THERE SHALL BE A FLOOR OR LANDING ON EACH SIDE OF THE DOOR. THE FLOOR LANDING SHALL NOT BE MORE THAN 1/2" LOWER THAN THE THRUSH HOLD OF THE DOORWAY.
8. THE PRECISE DIMENSIONS AND LOCATIONS OF ALL DOOR LOUVERS AND WINDOW OPENINGS SHALL BE DETERMINED FROM ARCHITECTURAL PLANS AND DETAILS. OTHER WALL AND FLOOR OPENINGS AS REQUIRED BY MECHANICAL, ELECTRICAL, OR SIMILAR REQUIREMENTS SHALL BE VERIFIED FROM SHOP DRAWINGS, EQUIPMENT DATA, ETC. AS REQUIRED.
9. DOOR OPENINGS NOT LOCATED BY DIMENSIONS SHALL BE CENTERED IN WALLS AS SHOWN OR LOCATED 5" FROM FINISH WALL TO FINISH JAMB UNLESS OTHERWISE NOTED.
10. SEE ARCHITECTURAL CEILING PLANS FOR DIMENSIONS LOCATED LIGHT FIXTURES, DIFFUSERS, AND SPEAKERS. ARCHITECTURAL REFLECTED CEILING PLANS DO NOT INDICATE WALL MOUNTED FIXTURES. REFER TO ELECTRICAL DRAWINGS FOR ALL LIGHTING FIXTURE - RECESSED, SURFACE, OR WALL MOUNTED.
11. CEILING SUSPENSION SYSTEM SHALL PROVIDE FOR CEILING SYSTEM ONLY. ADDITIONAL INDEPENDENT FRAMING FOR LIGHTING FIXTURE, EXIT SIGNS, GRILLES, AIRBARS, AND AIR CONDITIONING DIFFUSERS SHALL BE REQUIRED. ATTACHMENT OF HANGERS OR FRAMING TO DUCTWORK IS PROHIBITED.
12. WHERE LARGER STUDS OR FURRING ARE REQUIRED TO COVER DUCTS, PIPING, CONDUITS, ETC., THE LARGER STUD SIZE OR FURRING TO EXTEND FULL WIDTH OF WALL SURFACE WHERE FURRING OCCURS.
13. PROVIDE ADEQUATE ANCHORAGE, BLOCKING, BACKING, AND FRAMING FOR FIRE SPRINKLERS, PIPING, LIGHT FIXTURES, ELECTRICAL UNITS, HVAC EQUIPMENT, DRAPERY, AND CEILING TRACKS AS REQUIRED FOR A COMPLETE INSTALLATION.
14. ALL CABINETS RECESSED INTO CORRIDOR WALLS SHALL BE BACKED WITH ONE HOUR OR TWO HOURS FIRE RESISTIVE CONSTRUCTION AS REQUIRED. ELECTRICAL PANELS AND/OR WIRING SYSTEMS SHALL NOT BE LOCATED IN CORRIDOR OR SHAFT WALLS.
15. ALL STEEL STUD SIZES AND SPACING IN GYPSUM BOARD WALLS SHALL BE IN ACCORDANCE WITH UNDERWRITER LABORATORIES, INC. FOR RATED ASSEMBLIES. SUBMIT DATA FOR APPROVAL.
16. ALL SINGLE LAYER GYPSUM BOARD WALLS CONTINUOUS AND CONTIGUOUS WITH DOUBLE LAYER GYPSUM BOARD SHALL MAINTAIN ONE CONTINUOUS OUTER LAYER OF GYPSUM BOARD AT THE SAME FACE OF FINISH. METAL STUDS AND FURRING CHANNELS SHALL BE OFFSET ACCORDINGLY.
17. WALL AND COLUMN GYPSUM BOARD FACING ON OTHER THAN FIRE AND SOUND RATED WALLS SHALL EXTEND 6" MINIMUM ABOVE CEILING HEIGHT.
18. GYPSUM BOARD ON INTERIOR METAL STUDS SHALL BE 5/8" THICK UNLESS NOTED OTHERWISE.

**MECHANICAL AND PLUMBING**

1. CEILING ACCESS PANELS SHALL BE PROVIDED BY THE MECHANICAL FIRE SPRINKLER AND PLUMBING CONTRACTORS AND LOCATED BELOW ALL VALVES, DUCTWORK, FIRE DAMPERS, ETC., AND AS REQUIRED OR AS DIRECTED BY THE ARCHITECT.
2. FIRE SPRINKLERS SHALL BE REVIEWED AND APPROVED BY THE FIRE MARSHALL AND DESIGN TEAM PRIOR TO INSTALLATION.
3. PIPE SLEEVES IN MECHANICAL EQUIPMENT ROOMS SHALL EXTEND 2" ABOVE THE FLOOR LINE. ELECTROLYSIS PROTECTION SHALL BE PROVIDED BETWEEN ALL DISSIMILAR METALS WHEREVER THE TWO ARE IN CONTACT.

**TYPICAL NOTES**

1. "SIMILAR" MEANS COMPARABLE CHARACTERISTICS FOR THE CONDITIONS NOTED. VERIFY DIMENSIONS AND/OR ORIENTATIONS ON PLAN AND/OR ELEVATIONS.
2. DIMENSIONS ARE NOT ADJUSTABLE WITHOUT APPROVAL OF ARCHITECT IN WRITING.
3. UNDERCUT ALL DOORS TO CLEAR TOP OF CARPET 3/4" MAXIMUM.

**CONTINUALLY OF ABBREVIATION:**

WDW. WINDOW	YD. YARD
W/O WITHOUT	& AND
W.P. WATERPROOF	@ AT
WNSCT. WAINSCOT	¢. CENTERLINE
WT. WEIGHT	Ø. DIAMETER OR ROUND
W.W.F. WELDED WIRE FABRIC	#. ROUND OR NUMBER
W/R WATER RESISTANT	*. SQUARE FOOT (FEET)

**SYMBOLS:**

	COLUMN LINE
	OTHER
	DOOR SYMBOL
	WORK POINT, CONTROL POINT, OR DATUM POINT
	REVISION
	CLOUD AND REVISION
	SECTION
	SECTION IDENTIFICATION
	DRAWING WHERE DETAIL IS DRAWN
	DETAIL
	DETAIL IDENTIFICATION
	DETAIL BACK REFERENCE
	INTERIOR ELEVATION(S)
	ELEVATION IDENTIFICATION (UNFOLD ELEVATIONS CLOCKWISE)
	DETAIL BACK REFERENCE
	ROOM IDENTIFICATION
	ROOM NAME
	ROOM NUMBER
	USE

**MATERIALS:**

	EARTH
	ROCK FILL
	SAND/ MORTAR/ PLASTER
	CONCRETE COST-IN-PLACE
	WOOD FRAMING THROUGH MEMBERS
	WOOD FRAMING INTERRUPTED MEMBERS
	WOOD, FINISH
	PLYWOOD
	GLASS
	ACOUSTICAL TILE OR BOARD
	GYPSUM BOARD
	INSULATION, BATT

**PARKING:**

**EXISTING:**  
EXISTING STANDARD PARKING SPACES:10  
EXISTING ADA ACCESSIBLE PARKING SPACE:01  
FUEL CANOPY PARKING:04  
TOTAL EXISTING PARKING SPACES:15

**REQUIRED:**  
TOTAL BUILDING AREA 1680 SF/300 = 6 PARKING

**PROPOSED:**  
PROPOSED STANDARD PARKING SPACES:04  
PROPOSED EV PARKING SPACE:01  
EXISTING ACCESSIBLE PARKING SPACE:01  
AIR AND WATER MACHINE PARKING SPACE:01  
FUEL CANOPY PARKING:04  
TOTAL PROVIDED:11

**OCCUPANT LOAD:**

Building area = 1,680 SF  
occupant load = 150 SF per person  
According to Chapter 1004.4 table 1004.5  
Total occupant =15

**DRAWINGS SHEET INDEX:**

SHEET NUMBER	SHEET TITLE
T-1.0	TITLE AND CODE SHEET

**GENERAL:**  
G-1.0 GENERAL INFORMATION

**CIVIL:**  
C-01 EXISTING SITE PLAN  
C-02 PROPOSED SITE PLAN  
C-3.0 ACCESSIBLE PARKING  
C-4.0 ACCESSIBLE COMPLIANCE RESTROOM

**ARCHITECTURAL:**

A-1.0 EXISTING FLOOR PLAN AND ELEVATION  
A-2.0 PROPOSED BUILDING PLAN AND ELEVATION  
A-2.1 ENLARGED FLOOR PLAN  
A-3.0 TYPICAL RESTROOM DETAILS  
A-4.0 SIGNAGE DETAIL  
A-5.0 EXISTING GAS STATION PLAN AND ELEVATION  
A-6.0 PROPOSED GAS STATION PLAN AND ELEVATION

**ELECTRICAL:**

ED-1.0 GENERAL DETAIL  
E-1.0 PROPOSED ELECTRICAL SITE PLAN  
E-2.0 PHOTOMETRIC SITE PLAN

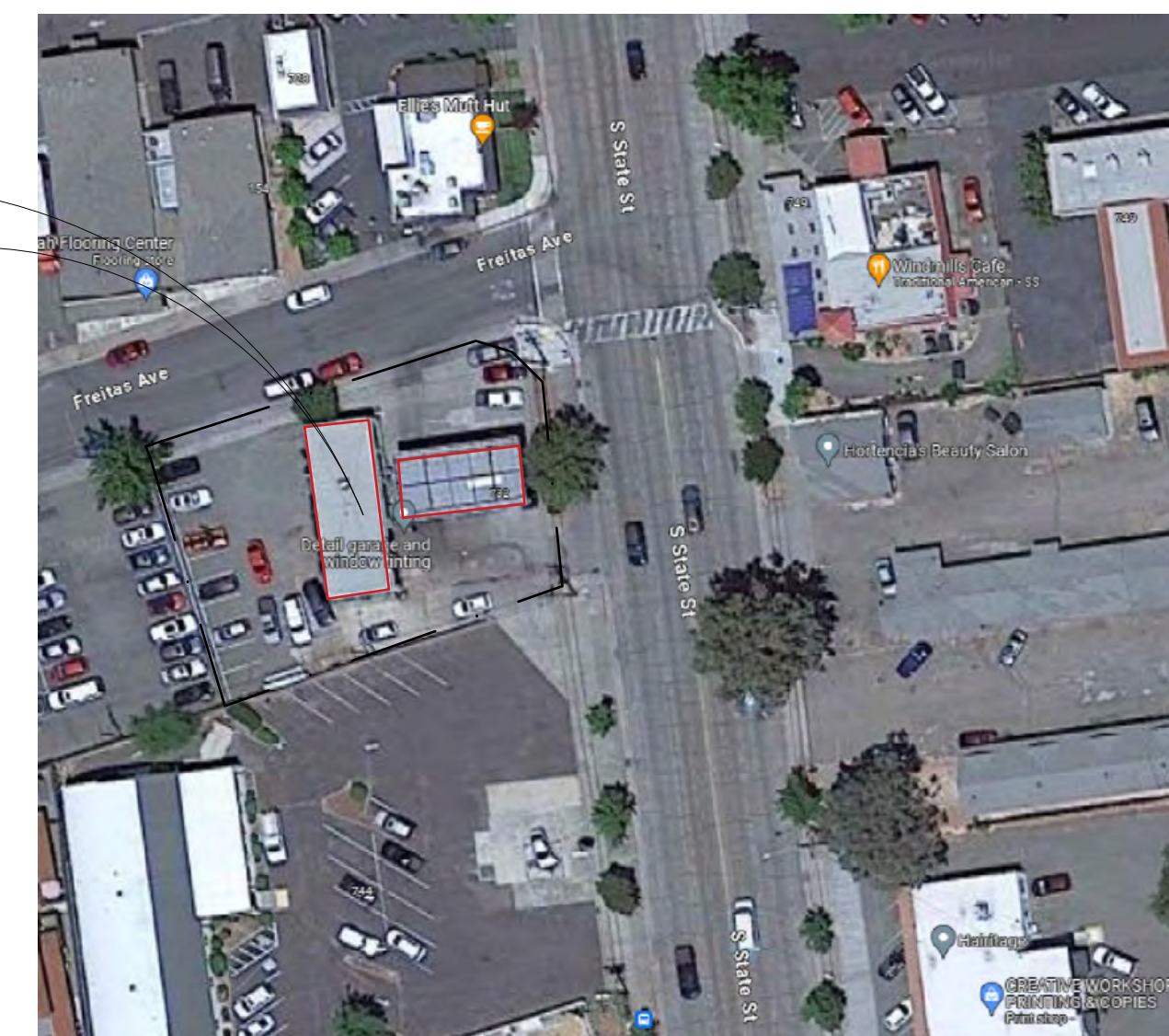
**APPLICABLE CODES:**

2022 CALIFORNIA BUILDING CODE  
2022 CALIFORNIA MECHANICAL CODE  
2022 CALIFORNIA ELECTRICAL CODE  
2022 CALIFORNIA PLUMBING CODE  
2022 CALIFORNIA FIRE CODE  
2022 CALIFORNIA ENERGY CODE  
2022 CALIFORNIA GREEN BUILDING STANDARDS UKIAH CITY STANDARD CODE

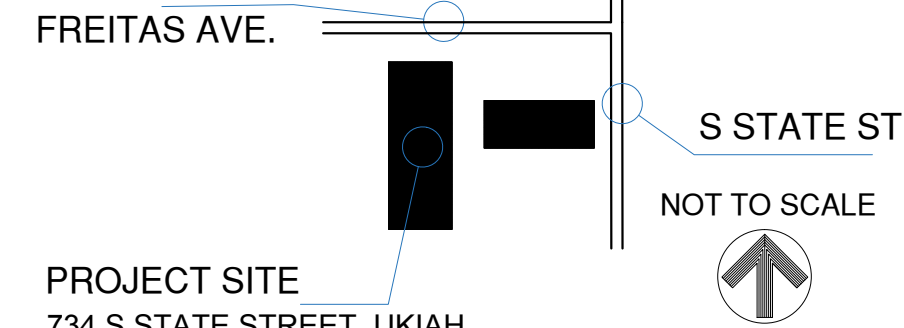
**AUTHORITIES HAVING JURISDICTION:**

CITY OF Ukiah, CA 95482

734 S STATE ST,  
UKIAH, CA 95482



**VICINITY MAP:**



**PROJECT SITE**  
734 S STATE STREET, UKIAH,  
CA 95482

**PROJECT TEAM:**

**OWNER:**

BP WALIA INC  
Address: 498 BELTRAMI DR  
Ph: 707 - 529-5239  
Email: jaswinderwalia105@gmail.com

**APPLICANT/ENGINEER:**

BOULEVARD CONSTRUCTION INC.]

4080 Truxel Road  
Sacramento, CA 95834  
Phone: 916-529-3982  
pm@theboulevard.us

**PROJECT INFORMATION:**

APN: 003-031-78-00  
ZONING: C1

**PROJECT DESCRIPTION:**

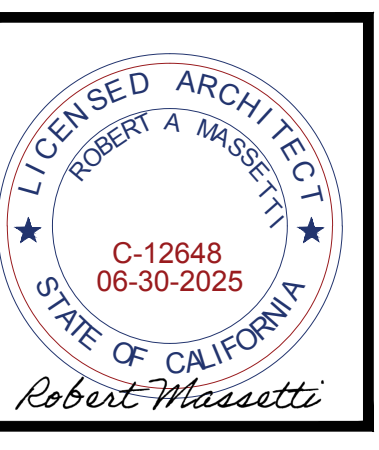
**DEMOLITION:**  
CLOSE TWO EXISTING ROLL-UP DOORS AND ONE SWING DOOR.  
REMOVE INTERIOR WALL, REMOVING REAR SIDE WINDOWS

**CONSTRUCTION:**

- INSTALL A NEW WALK-IN COOLER.
- ADD A NEW KITCHEN (for further details refer sheet A-2.1)
- INSTALL A NEW SALES COUNTER.
- PARKING LOT IMPROVEMENTS
- UPGRADE EXISTING MONUMENT SIGN.
- UPGRADING LANDSCAPE
- INSTALL A NEW FASCIA
- PAINT JOB ON GAS STATION CANOPY
- NEW BUILDING SIGN
- ADDING TRASH ENCLOSURE

**PROJECT AREA:**

EXISTING LOT AREA (SQ FT): = 13,916  
EXTENSION AREA (SQ FT): = 0  
PROPOSED KITCHEN (SQ FT): =256  
TOTAL BUILDING AREA (SQ FT): = 1,680  
FUEL CANOPY AREA (SQ FT):=1,175  
OCCUPANCY GROUP: M-1  
CONSTRUCTION TYPE: TYPE - V



**PROJECT DIRECTOR:**  
FARAH CHAUDHARY  
PHONE:+1 (916) 793-4048  
Farah@theboulevard.us

**PROJECT MANAGER:**  
M.HAMAYOON  
PHONE:+1 (916) 477-0102  
hamayoon@theboulevard.us



**ARCHITECT OF RECORD:**



4080 TRUXEL ROAD, SUITE 100,  
SACRAMENTO, CA. US 95834  
Website: https://theboulevard.us  
Call: +1(916) 330-4200  
FAX NO:1(916) 914-2215  
Email: info@theboulevard.us

PROJECT:  
734 SOUTH STATE STREET UKIAH,  
CA 95482

REVISIONS:	DATE:	DESCRIPTION:

CURRENT ISSUE DATE:

01,24,2024

ISSUED FOR :

01,24,2024

CONTROL NO.:

DRAWN BY: BOULEVARD

CHECKED:

APPROVED:

SHEET TITLE

TITLE AND CODE SHEET

SHEET #

T-1.0

# GENERAL CONDITIONS

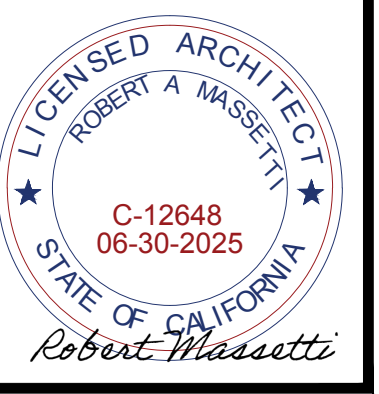
1. ALL WORK SHALL COMPLY WITH LOCAL CODES AND ORDINANCES. EXCLUSION OF SAID ITEMS FROM DRAWINGS OR SPECIFICATIONS DOES NOT RELEASE THE CONTRACTOR FROM COMPLIANCE WITH ABOVE MENTIONED CODES AND ORDINANCES.
2. VERIFY ALL CONDITIONS ON SITE. NOTIFY ARCHITECT IN WRITING OF ANY DISCREPANCY BETWEEN ACTUAL CONDITIONS AND THOSE SHOWN ON DRAWINGS.
3. ALL NAILING PER CBC CURRENT EDITION.
4. DOORS – EXTERIOR
  - A. HOLLOW METAL WITH PRESSED METAL FRAME.  
  
ALL EXTERIOR DOORS TO BE WEATHERSTRIPPED.

DOORS – INTERIOR:

  - A. PRE-HUNG VERTICAL GRAIN DOUGLAS FIR SOLID STILE AND RAIL MULTIPLE PANEL.
5. CASEWORK  
PROVIDE ALL WOOD CASEWORK AND CABINETS AS INDICATED COMPLYING WITH WIC MANUAL OF MILLWORK, LATEST EDITION FOR CUSTOM GRADE, FINISH AS DETERMINED BY OWNER. PARTICLE BOARD AND HARDWOOD PLYWOOD SHALL COMPLY WITH LOW FORMALDEHYDE EMISSION STANDARDS.
6. FLOOR TILE  
INSTALLATION PER MANUFACTURERS RECOMMENDATIONS.
7. WOOD – INTERIOR TRIM
  - A. INCLUDES ALL STANDING AND RUNNING WOOD TRIM MEMBERS WHICH ARE NOT STRUCTURAL IN NATURE, MILLWORK, INTERIOR JAMBS, AND RAILINGS. SPECIES: PINE, WIC GRADE CUSTOM, FINISH AS DETERMINED BY OWNER. MINIMUM STANDARDS FOR FINISH CARPENTRY AND MILLWORK SHALL COMPLY WITH MANUAL OF MILLWORK STANDARDS OF INDUSTRY ADOPTED BY WIC, LATEST EDITION. PARTICLE BOARD HARDWOOD PLYWOOD SHALL COMPLY WITH LOW FORMALDEHYDE EMISSIONS STANDARDS.
8. EXISTING HVAC  
SELECT EQUIPMENT PER ACCA 36-S AND SIZE DUCTS PER ACCA 29-D. VERIFY LOCATION OF REGISTERS PRIOR TO INSTALLATION. PROVIDE CERTIFICATION OF TITLE 24 ENERGY COMPLIANCE.
9. PAINT  
WOOD, AND RESTROOM WALLS AND CEILINGS: ALKYD ENAMEL, ALL OTHER WALLS AND CEILINGS: LATEX ENAMEL. ALL PAINTS, STAINS AND OTHER COATINGS SHALL COMPLY WITH VOC LIMITS.  
  
OAK  
OIL AND WAX SHALL COMPLY WITH VOC LIMITS.
10. VERIFY ALL ELECTRICAL FIXTURE LOCATIONS PRIOR TO INSTALLATION.
11. THE CONTRACTOR SHALL GUARANTEE ALL WORK IN GENERAL FOR ONE YEAR FROM THE DATE OF ACCEPTANCE BY THE OWNER. DEGREE OF DEFECTIVES TO BE DETERMINED BY THE OWNER.
14. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES OR FOR SAFETY PRECAUTIONS IN CONNECTION WITH THE WORK AND SHALL NOT BE RESPONSIBLE FOR THE CONTRACTOR'S FAILURE TO CARRY OUT CORDANCE WITH THE CONTRACT DOCUMENTS.

# ACCESSIBILITY NOTES

1. The architect has verified that the entire is graded/paved to fully complaint with the code accessibility requirements, including the maximum slope in any directions being 1.5% or less. Therefore, such site is construed to have accessible routes throughout the entire areas.
2. POT indicated on the site plan is the most practical and direct route between site arrival point and all exterior pedestrian doors at new building. Exact location of POT must be field verified, relocated or reconfigured if needed.
3. POT indicated on the site plan is also the most practical and direct route between accessible parking and all exterior pedestrian doors at new building. Exact location of POT must be field verified, relocated or reconfigured if needed.
4. POT must be Min. 50 inches wide with Min.80 inches head clearance.
5. If any POT is less than 60 inches wide, then passing space at least 60 inches x 60 inches must be located at reasonable intervals not to exceed 200 feet.
6. Any POT with continuous gradients must have clear level area with Max. 2% slope in all directions at least 5 feet in length at intervals of 400 feet max.
7. Except for any required clear areas, POT may have Max. 5% slope in the direction of travel and Max. 2% cross slope.
8. POT may have Max. 1/2 inch level change with Max. 1:2 slope. Level change not exceeding 1/4 inch may be vertical. Any level changes exceeding 1/2 or any slope in the direction of travel exceeding 5% must be accommodated by code – complaint curb ramp or ramp.
9. POT surfaces must be firm, stable, slip-resistant, w/o loose gravels, sand, chips, etc.
10. If any gratings are located in POT, grid openings in gratings must be limited to 1/2 inch in the direction of traffic flow.
11. Abrupt change in level exceeding 4 inches between POT and adjacent surfaces (excluding street and driveway) or feature must be identified by Min. 6-inch high warning curb, or by a guard or handrail with a guide rail centered Min.2 inches and Max. 4 inches above the surface of POT.
12. All Bldg. doors must have clear, unobstructed level areas on both sides with Max. 2.5% slope in any direction. Clear level area at door front approach in the direction of door swing must be min. 60 inches deep x 60 inches wide (including Min. 24 inches pass door strike edge) on the exterior side of exterior door, and Min.60 inches deep x 54 inches wide (including Min. 18 inches pass door strike edge) at other locations for single door, or Min 60 inches deep x double-door width for double doors.
13. Clear level area at door front approach in the direction opposite door swing must be Min. 48 inches deep x 36 inches wide, or Min. 48 inches deep x 48 inches wide (including Min. 12 inches pass door strike edge) if door has both latches and closer for single door, or Min 48 inches deep x double-door width for double door.
14. Any level changes at the doorway, including threshold thickness, must be max. 1/2 inch W/Max. 1:2 slope. Any level change not eeding 1/4 inch may be vertical.



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**ARCHITECT OF RECORD:**  
**BOULEVARD**  
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 4080 TRUXEL ROAD, SUITE 100,  
 SACRAMENTO, CA, US 95834  
 Website: https://theboulevard.us  
 Call: +1(916) 330-4200  
 FAX NO: (916) 914-2215  
 Email: info@theboulevard.us

PROJECT:  
 734 SOUTH STATE STREET UKIAH,  
 CA 95482

REVISIONS:	DATE	DESCRIPTION	BY:			
			1	2	3	4

CURRENT ISSUE DATE:  
 01,24,2024

ISSUED FOR :  
 01,24,2024

CONTROL NO: \_\_\_\_\_

DRAWN BY: BOULEVARD  
 CHECKED:  
 APPROVED:

SHEET TITLE  
**GENERAL NOTE**

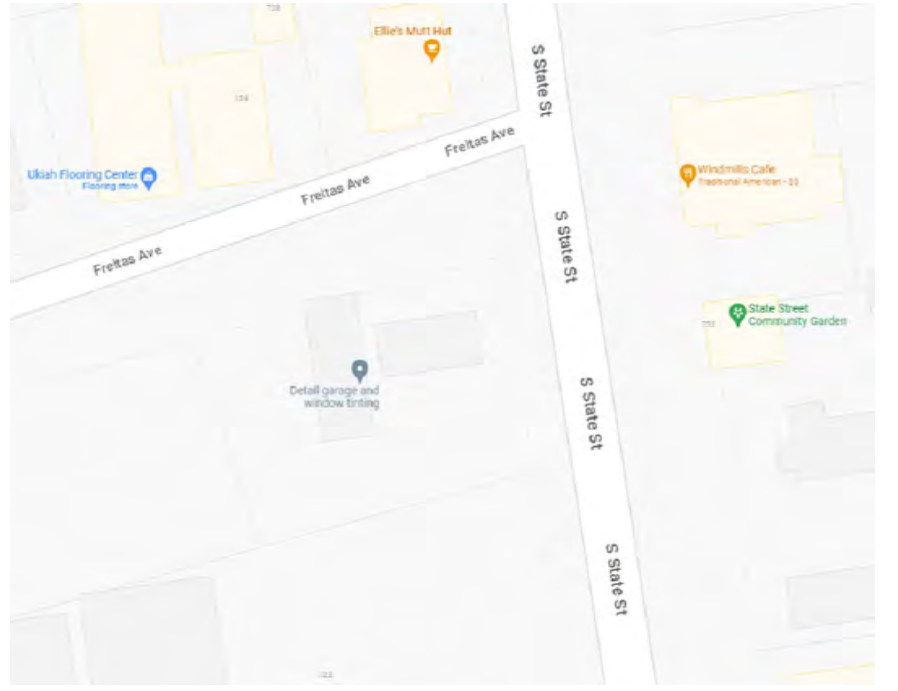
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**G-1.0**



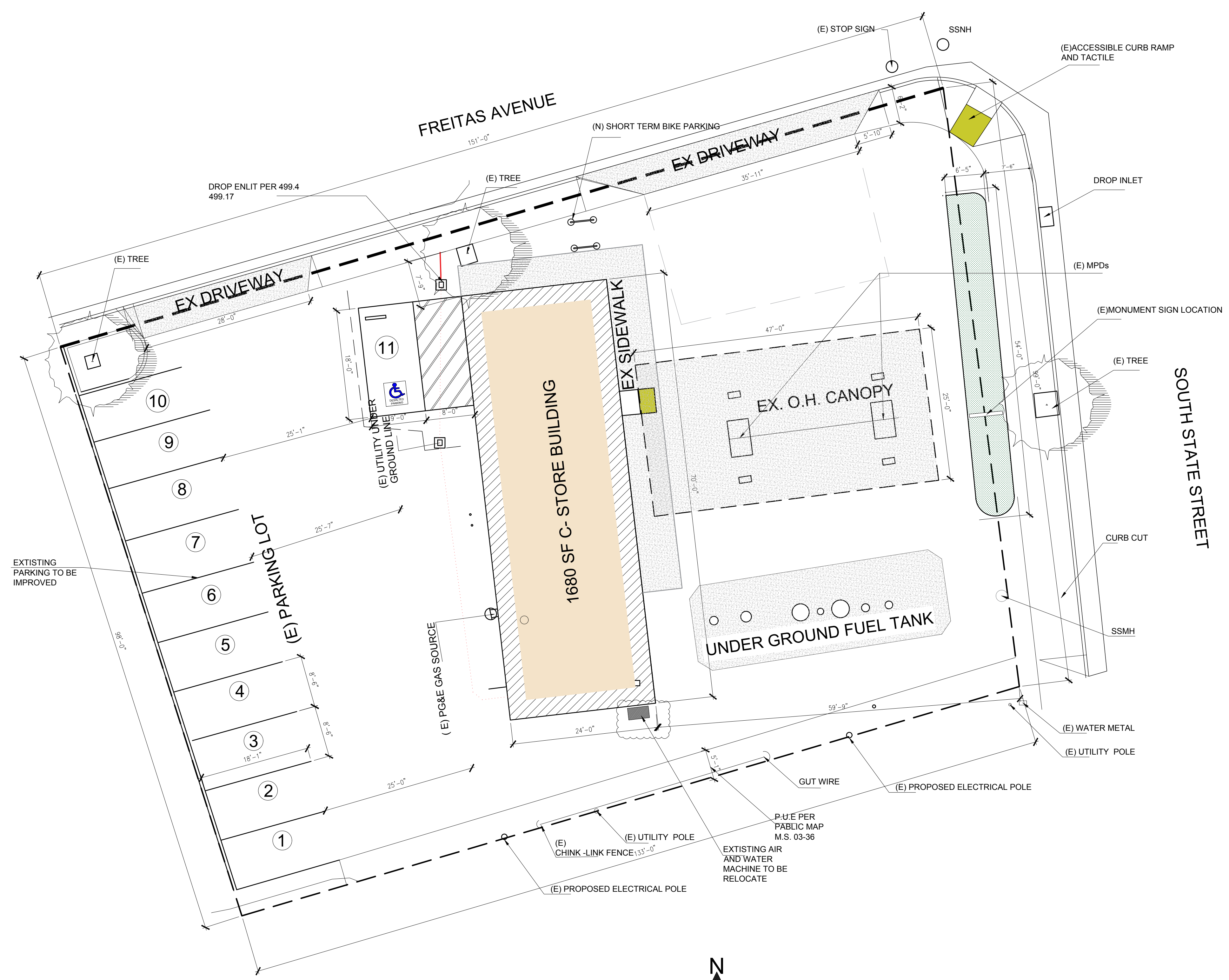
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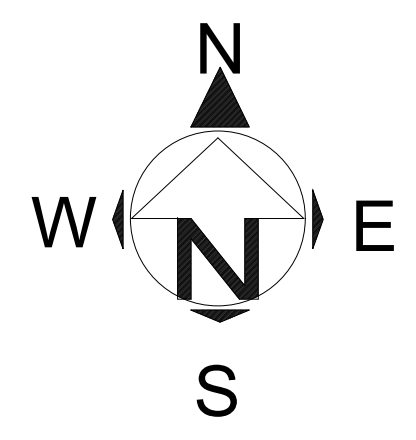
VICINITY MAP



LEGEND:

- PROPERTY LINE ————
- GAS STATION OVERHAND CANOPY LINE - - - -
- C- STOR BUILDING LINE // // //
- EXISTING DRIVEWAY [Pattern]
- EXISTING PLANTER [Pattern]
- EXISTING BUILDING SIDEWALK [Pattern]
- EXISTING TRESS [Symbol]
- EXISTING SEWER LINE LINE [Pattern]

1  
 C-2.0 A  
 EXISTING SITE PLAN  
 SCALE : 1" = 10'-0"



PROJECT:  
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 CA 95482

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 APPROVED:

SHEET TITLE  
 EXISTING SITE PLAN

SHEET #  
 C-1.0

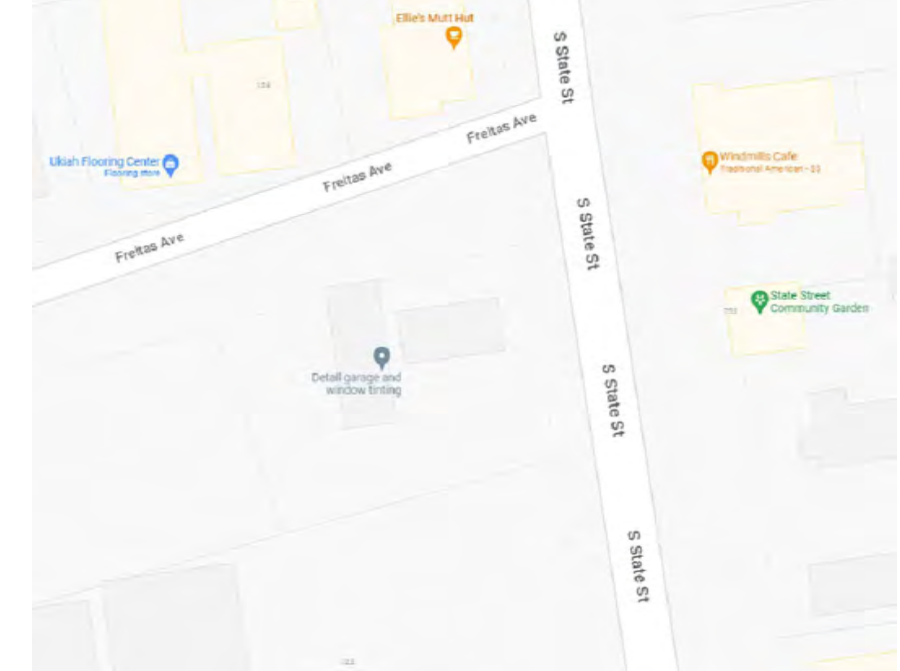


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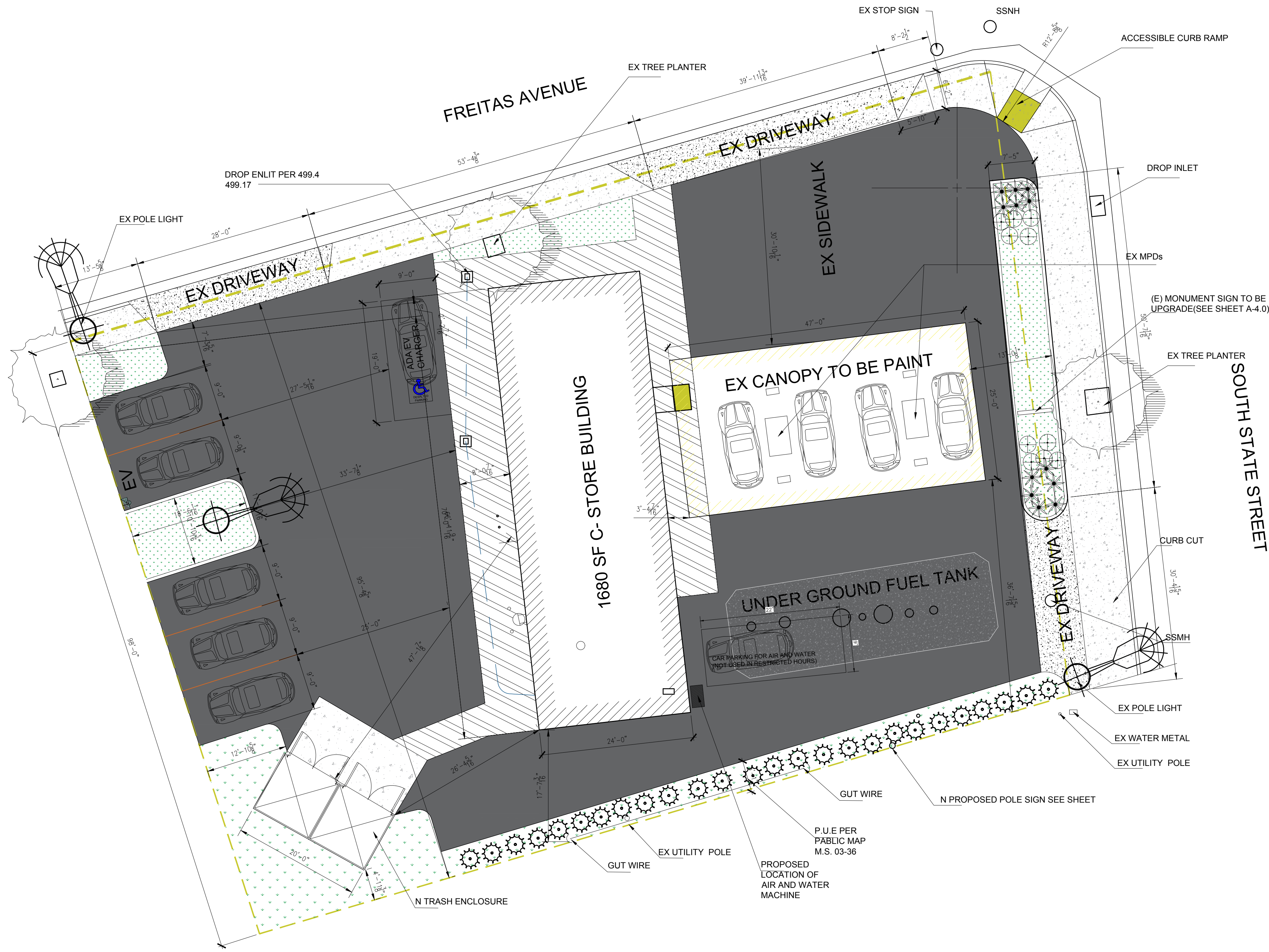
PROJECT:  
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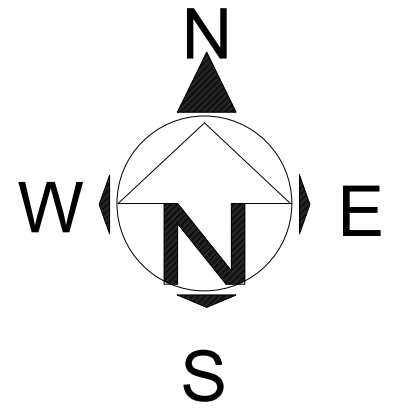
VICINITY MAP

LEGEND:

- PROPERTY LINE
- GAS STATION OVERHAND CANOPY LINE
- C- STOR BUILDING LINE
- EXISTING DRIVEWAY
- EXISTING PLANTER
- EXISTING BUILDING SIDEWALK
- EXISTING TRESS
- PROPOSED EV CHARGER SYMBOL
- PARKING STRIPS
- ADA TRAVEL PATH
- ADA ACCESSIBLE SYMBOL
- ASHPHALT



1  
 C-2.0 A  
 PROPOSED SITE PLAN  
 SCALE: 1" = 10'-0"



REVISIONS:	DATE:	DESCRIPTION:	BY:

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 01,24,2024

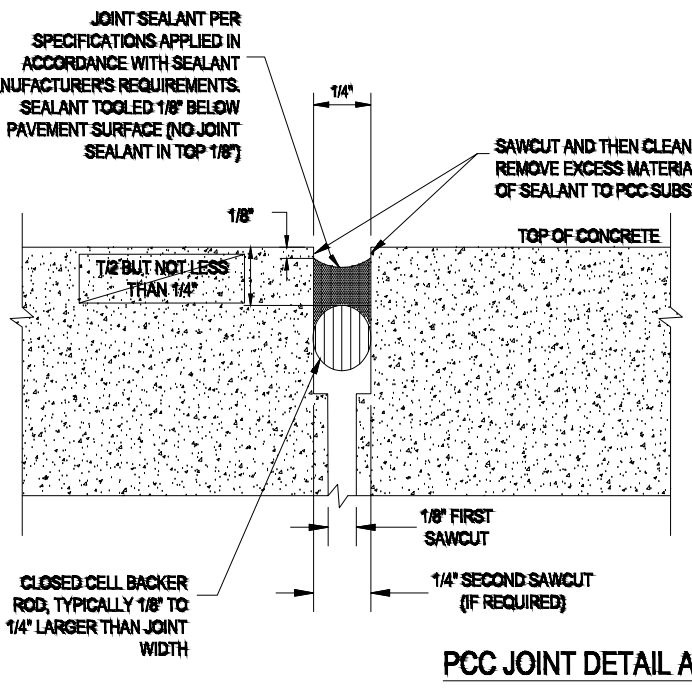
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CONTROL NO:

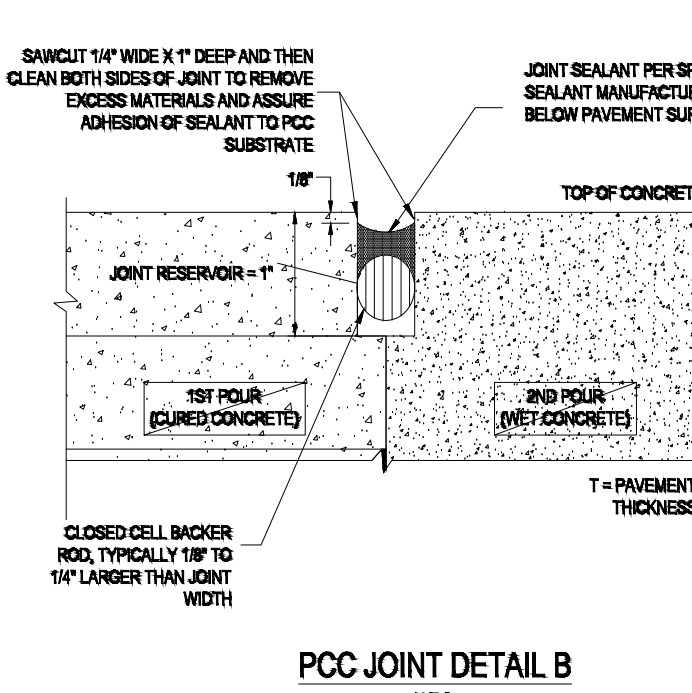
DRAWN BY: BOULEVARD  
 CHECKED:  
 APPROVED:

SHEET TITLE  
 PROPOSED SITE PLAN

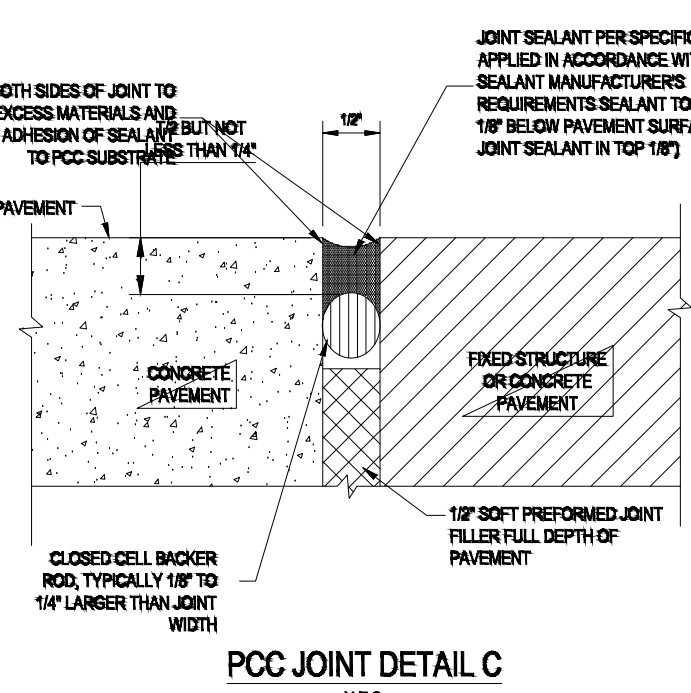
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 C-2.0



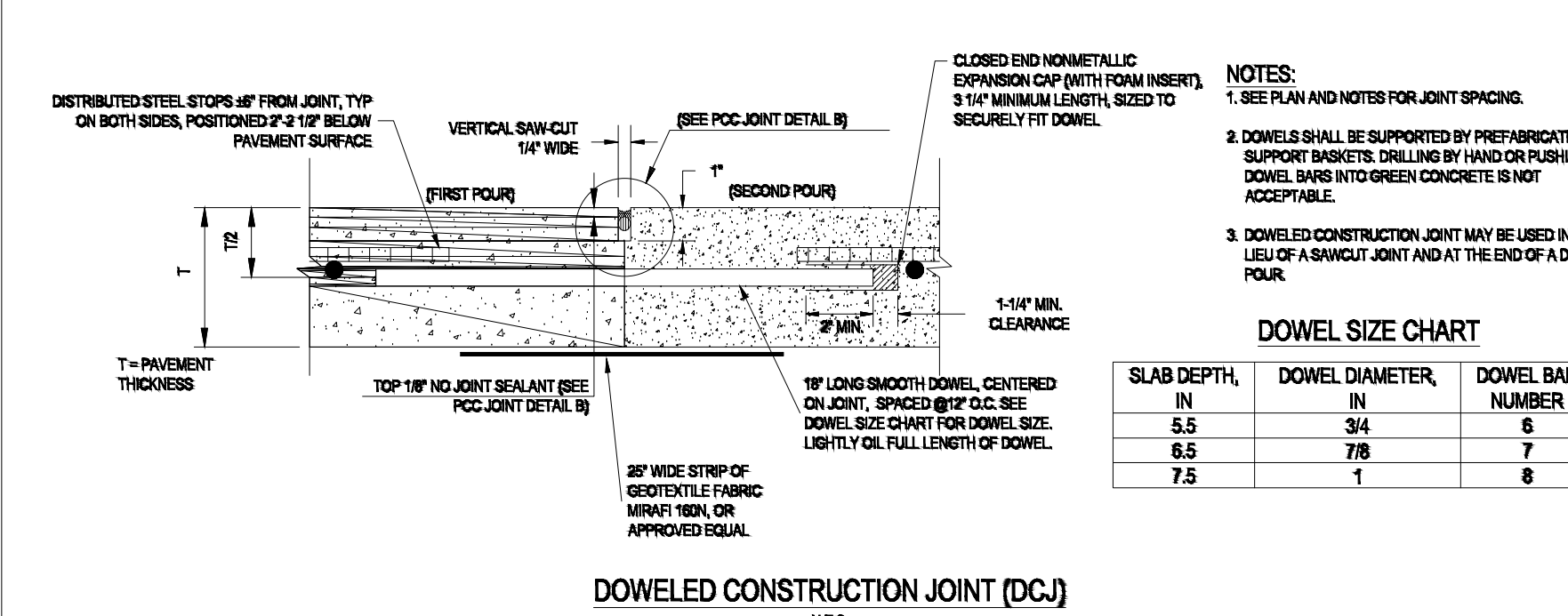
- NOTES:**
1. ASSURE SAW JOINTS ARE CLEAN AND DRY PRIOR TO THE APPLICATION OF THE JOINT SEALANT.
  2. INSTALL CLOSED CELL BACKER ROD AFTER JOINTS HAVE BEEN CLEANED AND DRIED IN ACCORDANCE WITH SEALANT MANUFACTURERS REQUIREMENTS.
  3. INSTALL BACKER ROD AT CONSISTENT AND UNIFORM DEPTH.
  4. JOINT SEALANT APPLICATION SHALL BE IN STRICT COMPLIANCE WITH SEALANT MANUFACTURERS REQUIREMENTS.



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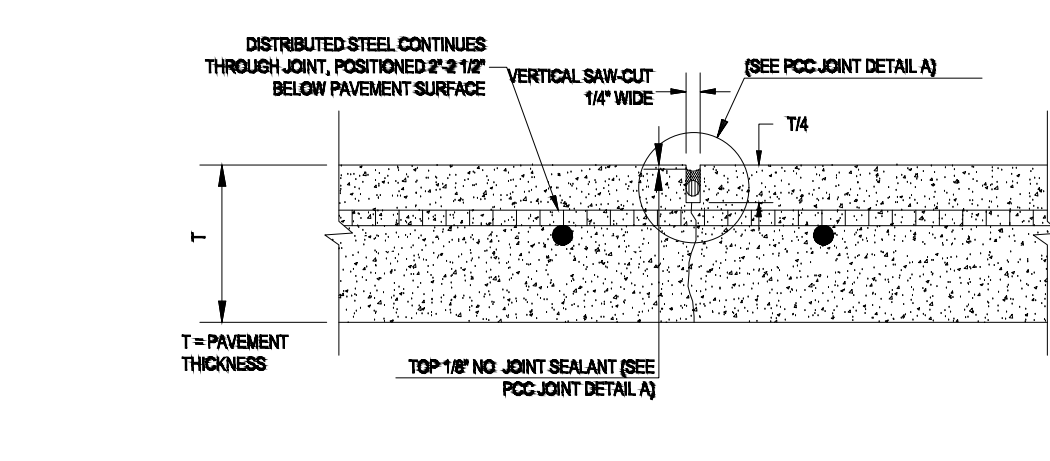
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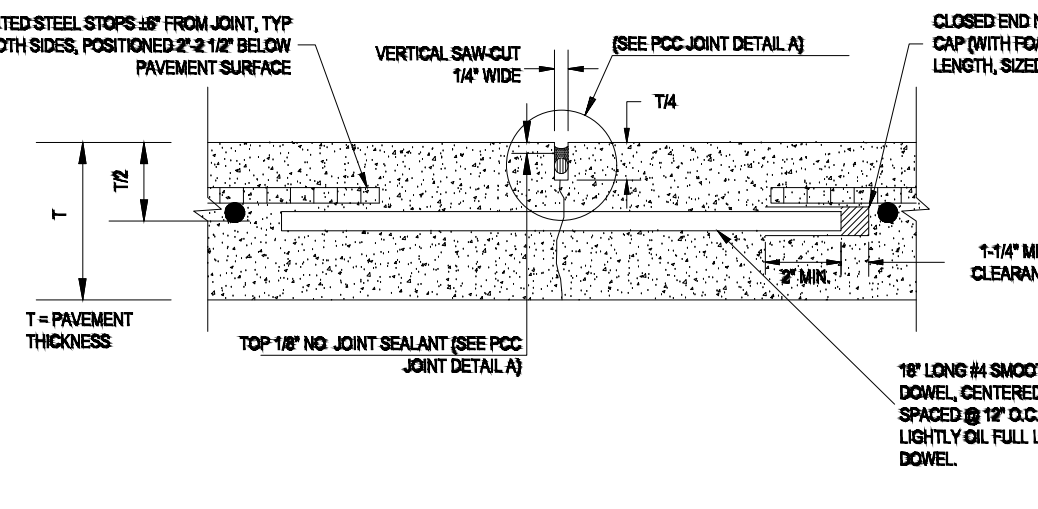
- NOTES:**
1. SEE PLAN AND NOTES FOR JOINT SPACING.
  2. DOWELS SHALL BE SUPPORTED BY PREFABRICATED SUPPORT BASKETS DRILLING BY HAND OR PUSHING DOWEL BARS INTO GREEN CONCRETE IS NOT ACCEPTABLE.
  3. DOWELED CONSTRUCTION JOINT MAY BE USED IN LIEU OF A SAWCUT JOINT AND AT THE END OF A DAYS POUR.
- DOWEL SIZE CHART**
- | SLAB DEPTH (IN) | DOWEL DIAMETER (IN) | DOWEL BAR NUMBER |
|-----------------|---------------------|------------------|
| 4.5             | 3/4                 | 6                |
| 5.5             | 7/8                 | 7                |
| 7.5             | 1                   | 8                |

**CONCRETE GENERAL NOTES**

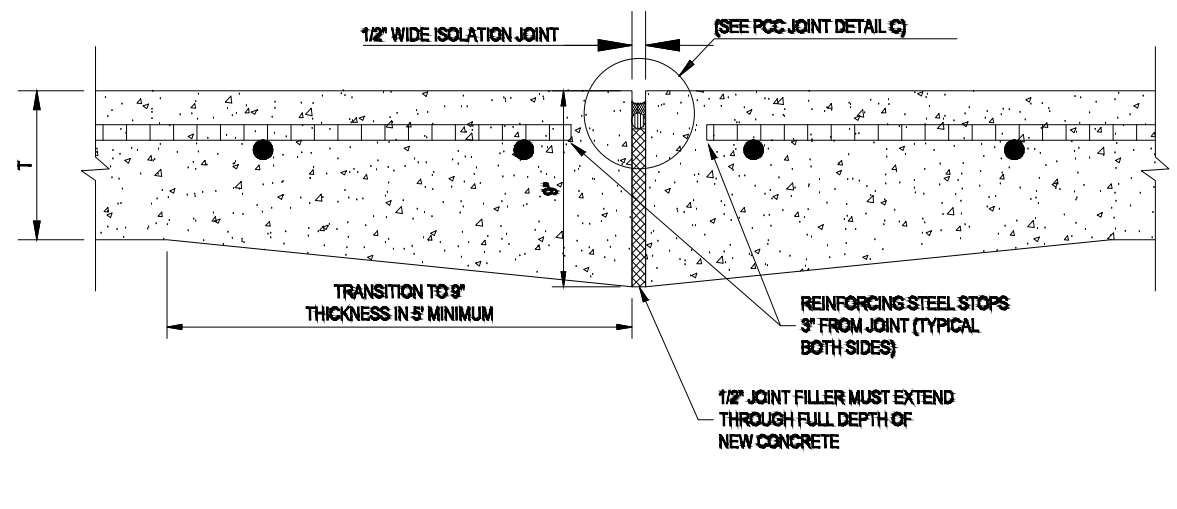
1. DETAILING OF CONCRETE REINFORCING BARS AND ACCESSORIES SHALL CONFORM TO THE RECOMMENDATIONS OF ACI 308 "DETAILS AND DETAILING OF CONCRETE REINFORCEMENT" AND ACI 309 "DETAILS AND DETAILING OF CONCRETE REINFORCEMENT".
2. REINFORCING BARS SHALL CONFORM TO THE RECOMMENDATIONS OF ACI 308 "MANUAL OF ENGINEERING AND PLACING DRAWINGS FOR REINFORCED CONCRETE STRUCTURES" AND ACI 309 "MANUAL OF STANDARD PRACTICE".
3. MIXING, TRANSPORTING AND PLACING CONCRETE SHALL CONFORM TO ACI 307. CONCRETE REINFORCEMENT BARS SHALL CONFORM TO ASTM A618, GRADE 60, NO. 3 BARS MAY CONFORM TO ASTM A618, GRADE 40, UNLESS NOTED OTHERWISE.
4. REINFORCEMENT BARS SHALL NOT BE TACK WELDED, WELDED, HEATED OR CUT UNLESS INDICATED ON THE CONTRACT DOCUMENTS OR REVIEWED BY THE STRUCTURAL ENGINEER.
5. REINFORCEMENT DESIGNATED AS "CONTINUOUS" SHALL LAP 30 BAR DIAMETERS AT SPLICES UNLESS NOTED OTHERWISE.
6. OBSERVE HOT & COLD WEATHER CONCRETING PRACTICES RECOMMENDED ON ACI 308 AND 309 RESPECTIVELY FOR EXTREME TEMPERATURES.
7. FORMWORK SHALL COMPLY WITH ACI 307, "RECOMMENDED PRACTICE FOR CONCRETE FORMWORK".
8. ALL CONCRETE DESIGN AND CONSTRUCTION SHALL BE IN ACCORDANCE WITH ACI 318.
9. ALL EXPOSED EDGES OF CONCRETE SHALL HAVE A 3/4 INCH, 45 DEGREE CHAMFER.
10. WELDED WIRE FABRIC SHALL CONFORM TO ASTM A641, FLAT MATS ONLY.
11. REFER TO JOB SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.



- NOTES:**
1. SEE PLAN AND NOTES FOR JOINT SPACING.
  2. SLAB MUST BE CUT AS SOON AS POSSIBLE AFTER PLACEMENT OF THE CONCRETE. THE SLAB SHOULD BE ABLE TO SUPPORT WEIGHT OF THE SAW WITHOUT TRACKING, AND THE SLAB SHOULD NOT RAVE. THE SLAB WHILE CUTTING, 12 HOUR DELAY BEFORE CUTTING WILL NOT BE ALLOWED.



- NOTES:**
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  2. DOWELS SHALL BE SUPPORTED BY PREFABRICATED SUPPORT BASKETS.
  3. SLAB MUST BE CUT AS SOON AS POSSIBLE AFTER PLACEMENT OF THE CONCRETE. THE SLAB SHOULD BE ABLE TO SUPPORT WEIGHT OF THE SAW, WITHOUT TRACKING, AND THE SLAB SHOULD NOT RAVE. THE SLAB WHILE CUTTING, 12 HOUR DELAY BEFORE CUTTING WILL NOT BE ALLOWED.



**GENERAL PAVING JOINT NOTES**

CONTROL JOINTS SHALL BE SAW CUT INTO THE CONCRETE WITHIN 12 HOURS AFTER CONCRETE IS POURED.

THE CONTROL JOINT PATTERN SHALL DIVIDE THE PAVEMENT INTO PANELS THAT ARE APPROXIMATELY SQUARE. THE LENGTH OF A PANEL MAY BE 50 PERCENT GREATER THAN THE WIDTH MAX.

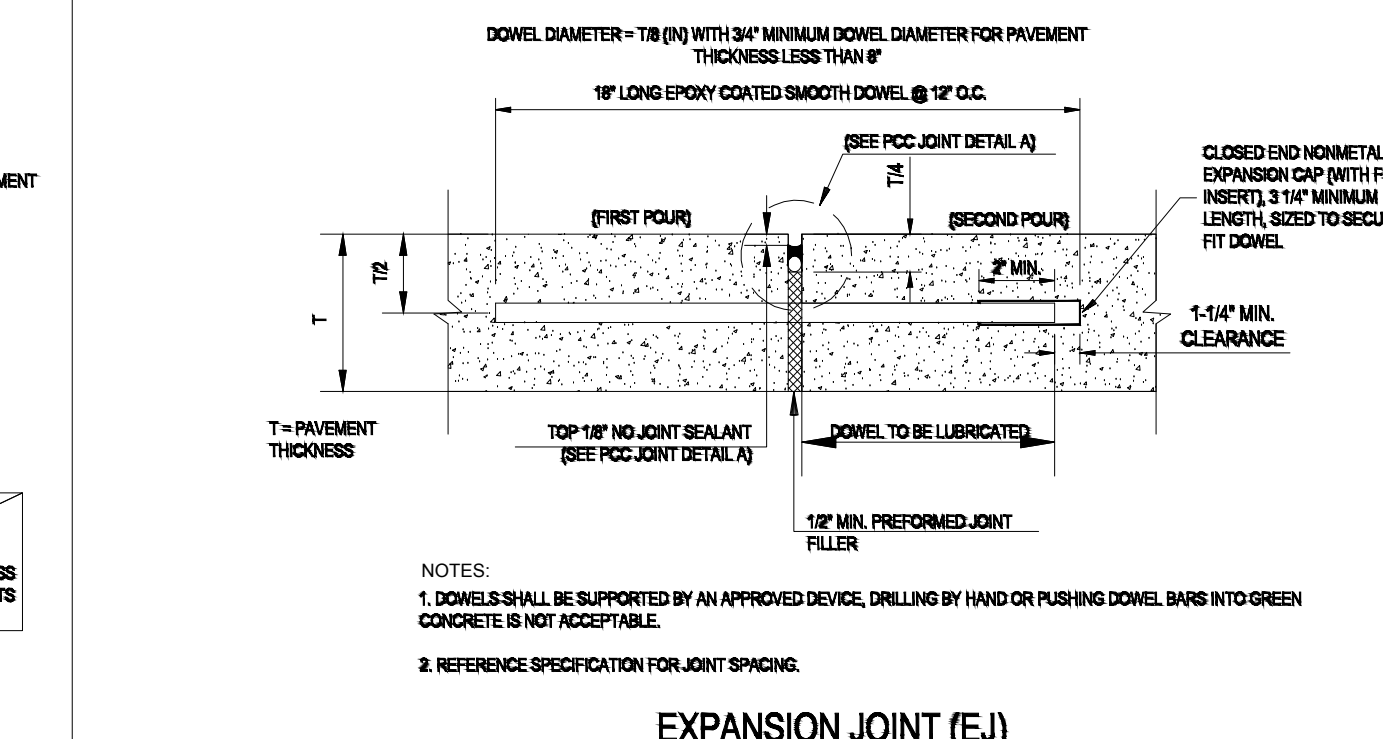
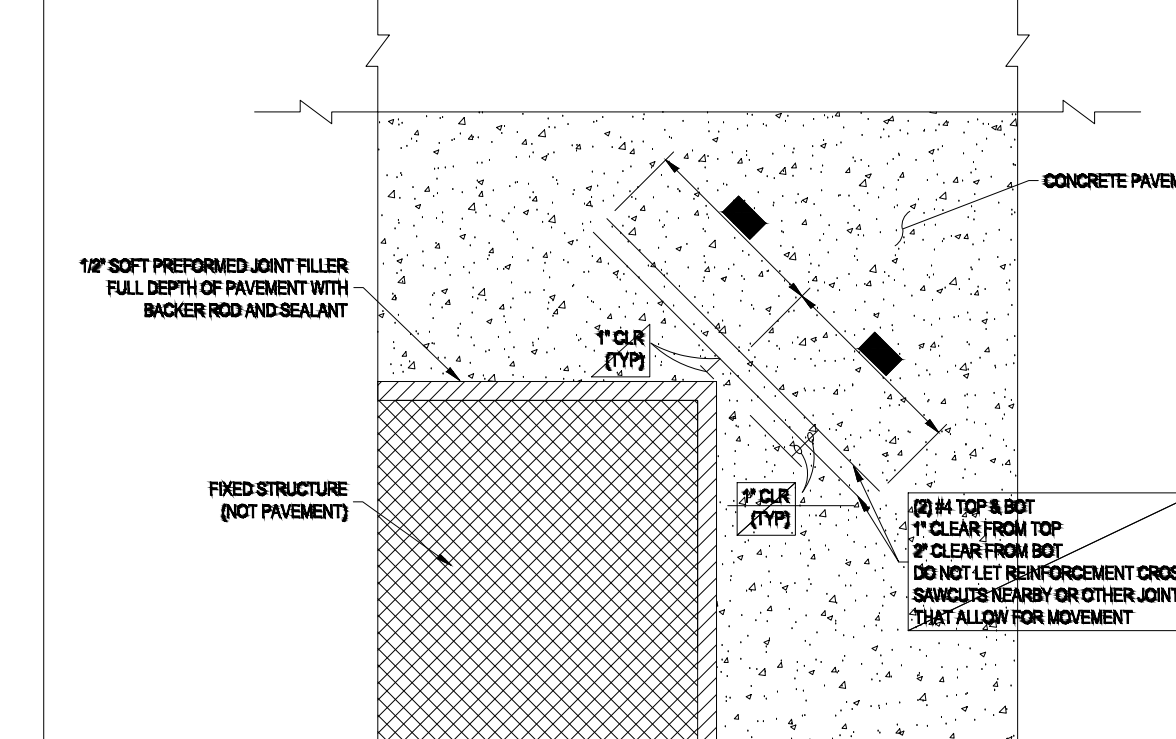
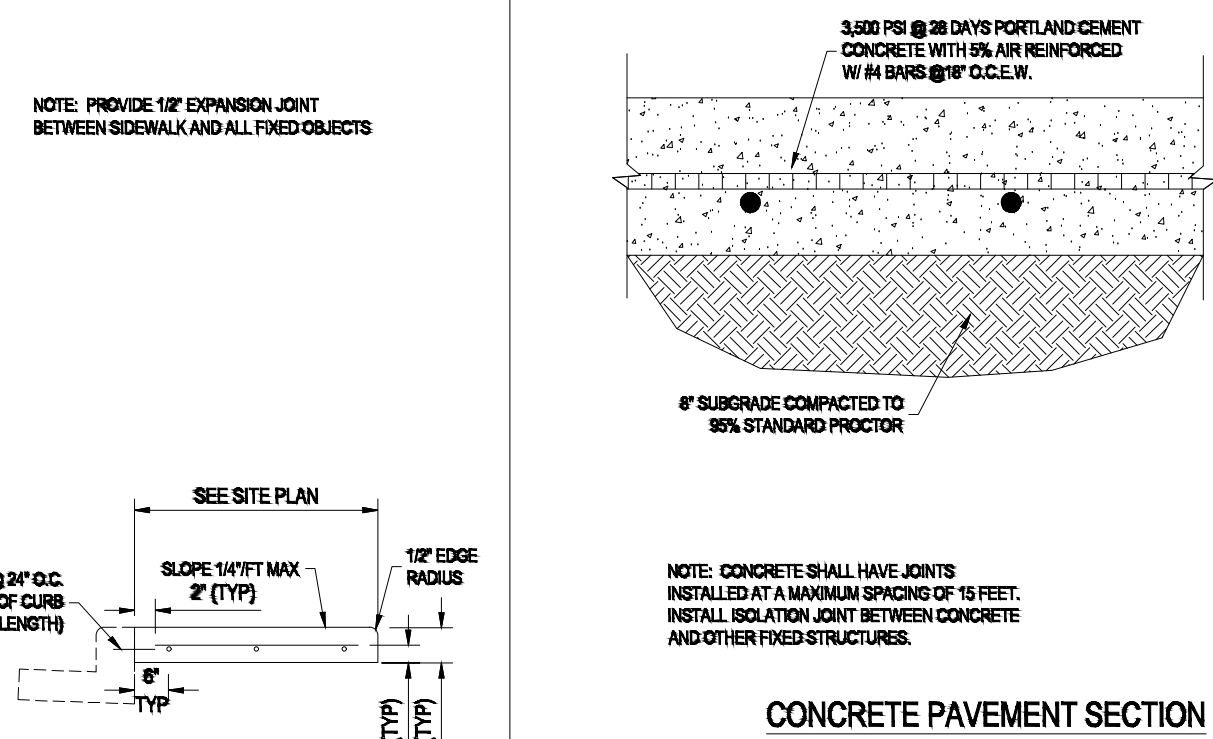
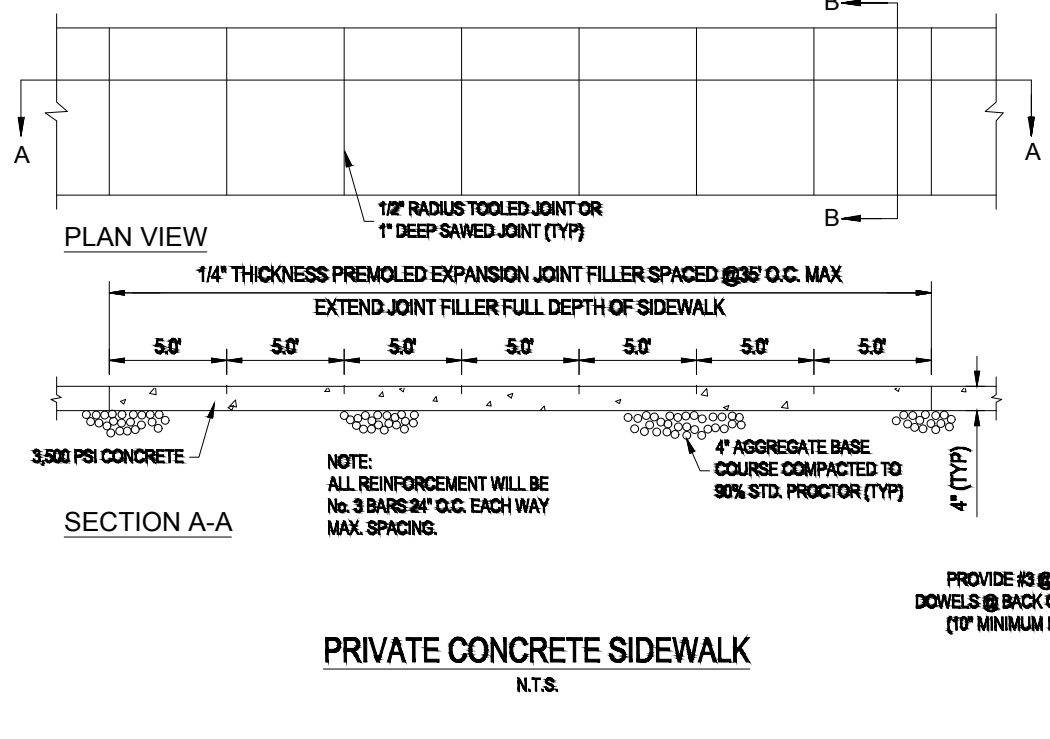
UNLESS OTHERWISE SHOWN ON PLANS, ABSOLUTE MAXIMUM JOINT SPACING SHALL BE PER CHART BELOW.

CONCRETE THICKNESS (INCHES)	MAXIMUM JOINT SPACING (FEET)
3.5	7.0
4.0	8.0
4.5	9.0
5.0	10.0
5.5	11.0
6.0	12.0
6.5	13.0
7.0	14.0
7.5	15.0
OVER 7.5	15.0

WHEN TRANSVERSE CONSTRUCTION JOINTS ARE NEEDED, THEY SHALL BE INSTALLED AT CONTRACTIONS/SAWCUT JOINT LOCATIONS IF POSSIBLE.

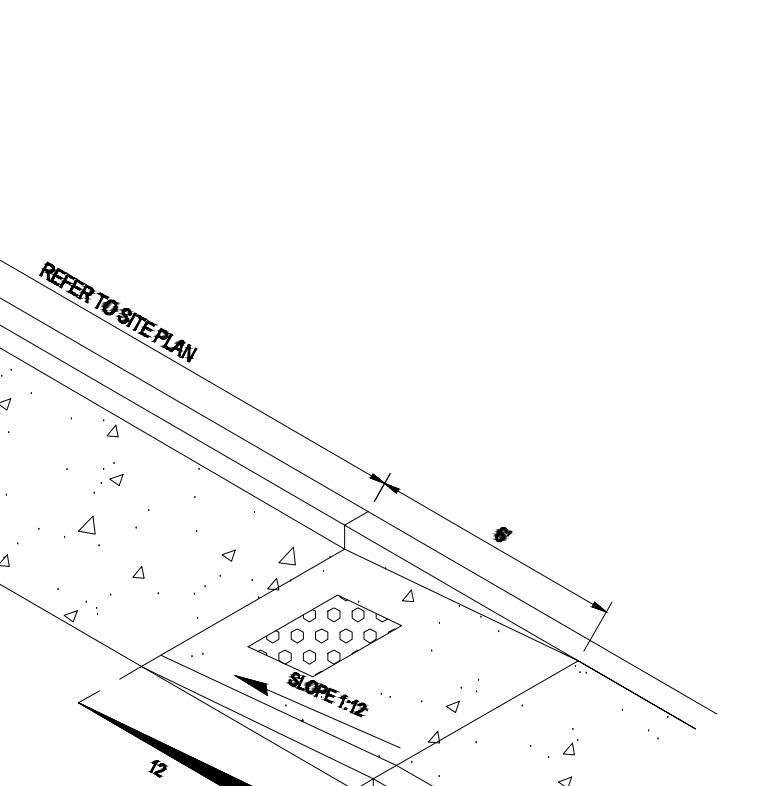
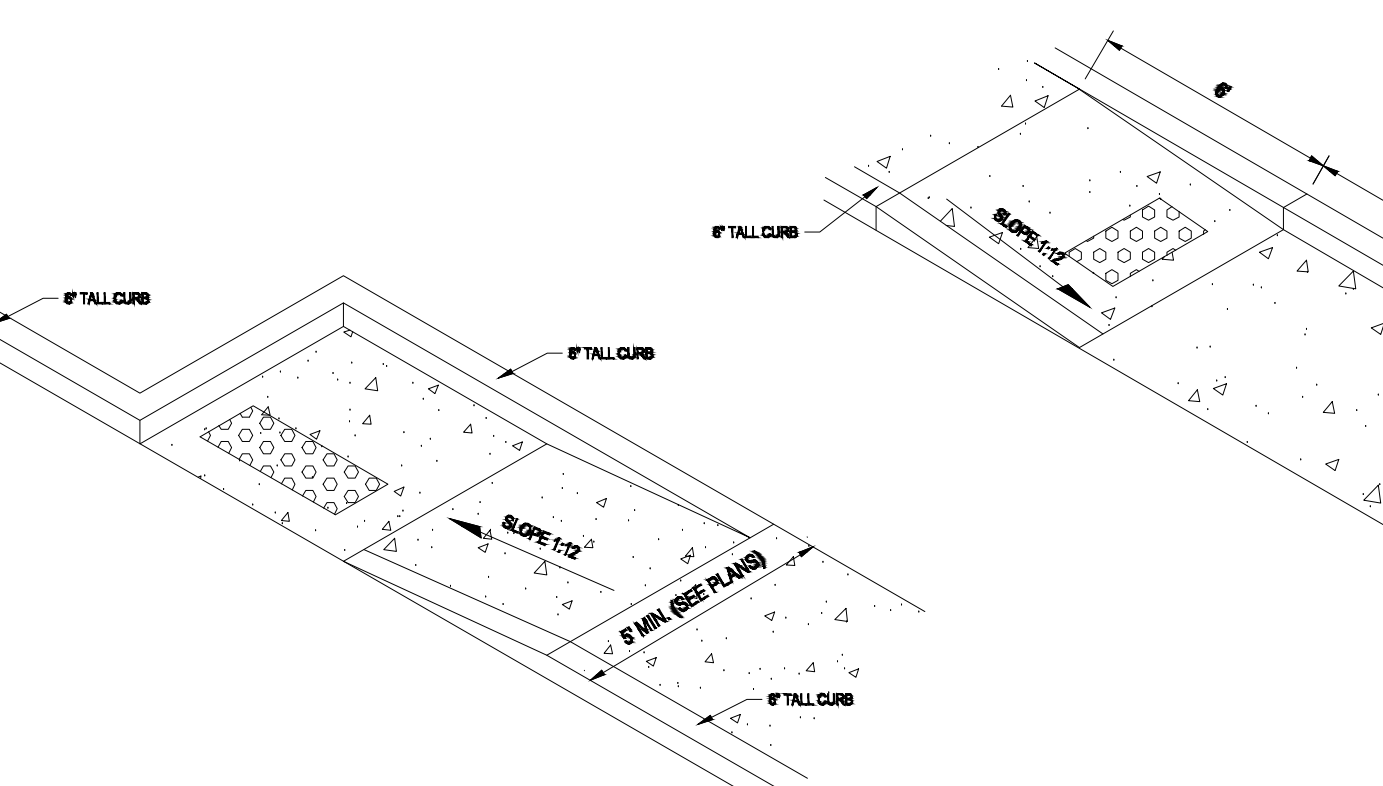
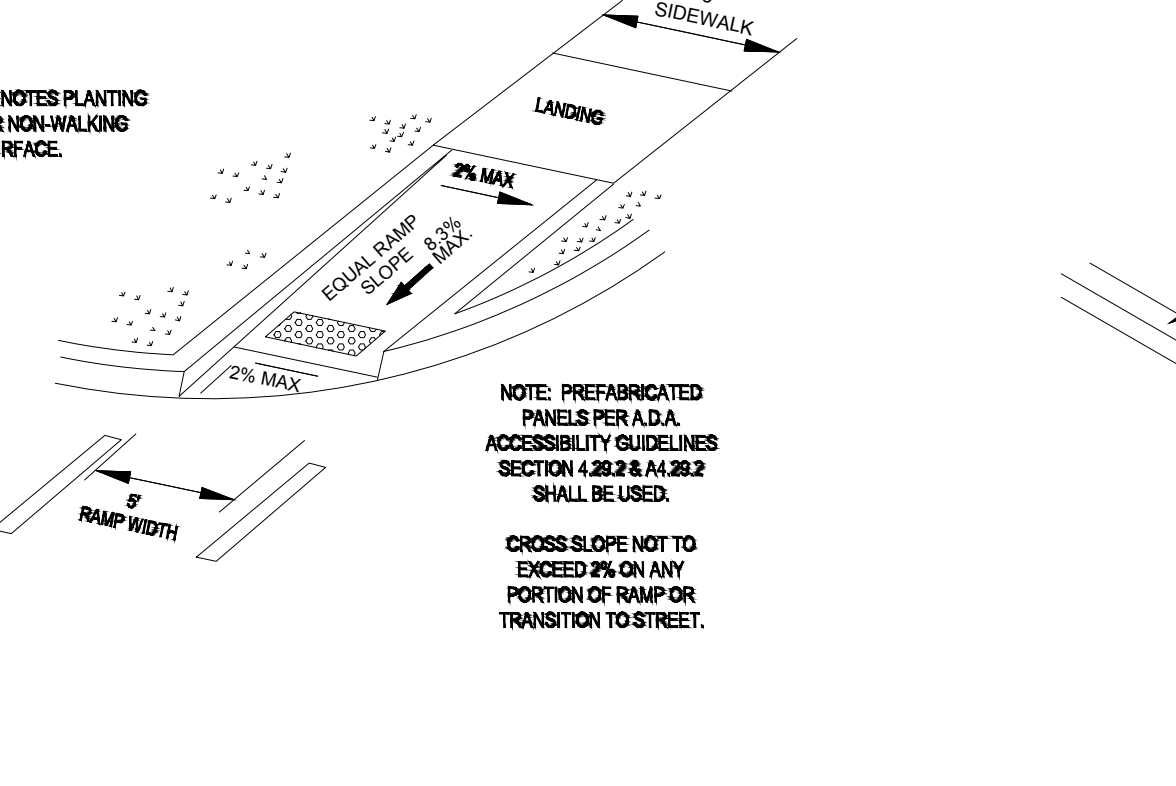
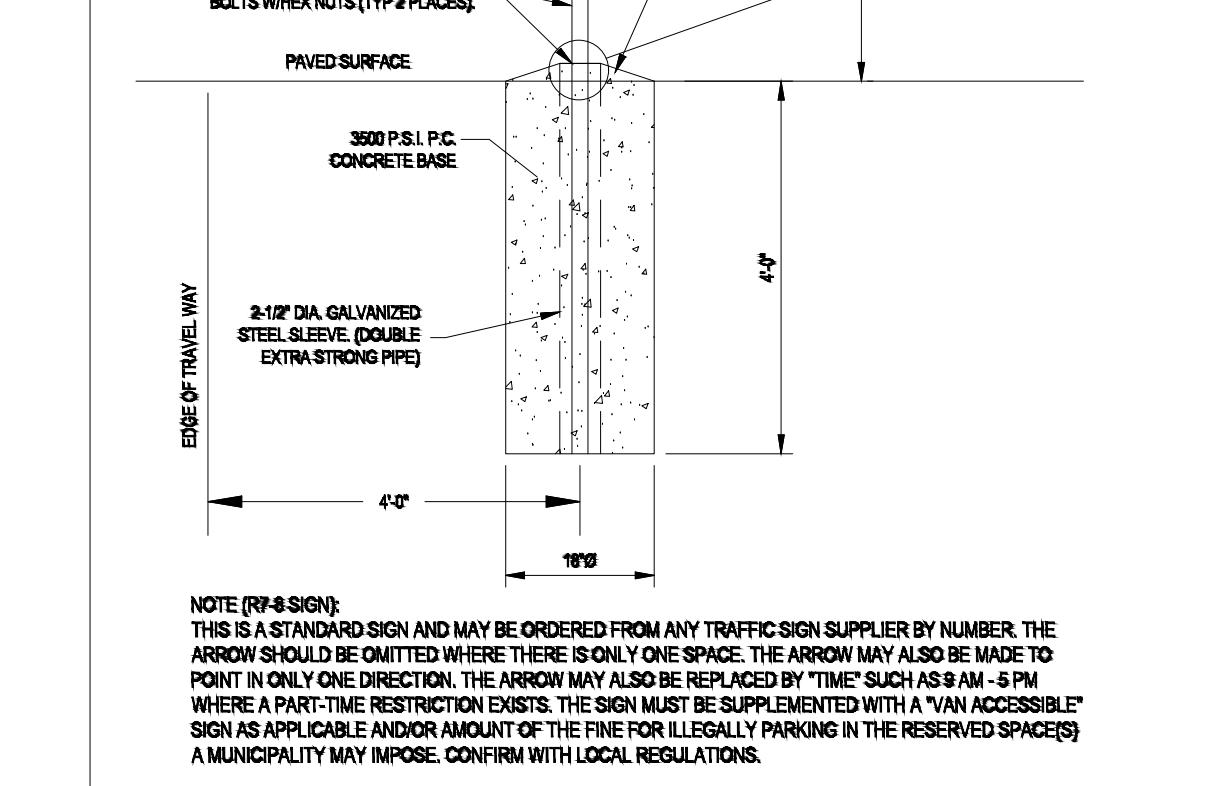
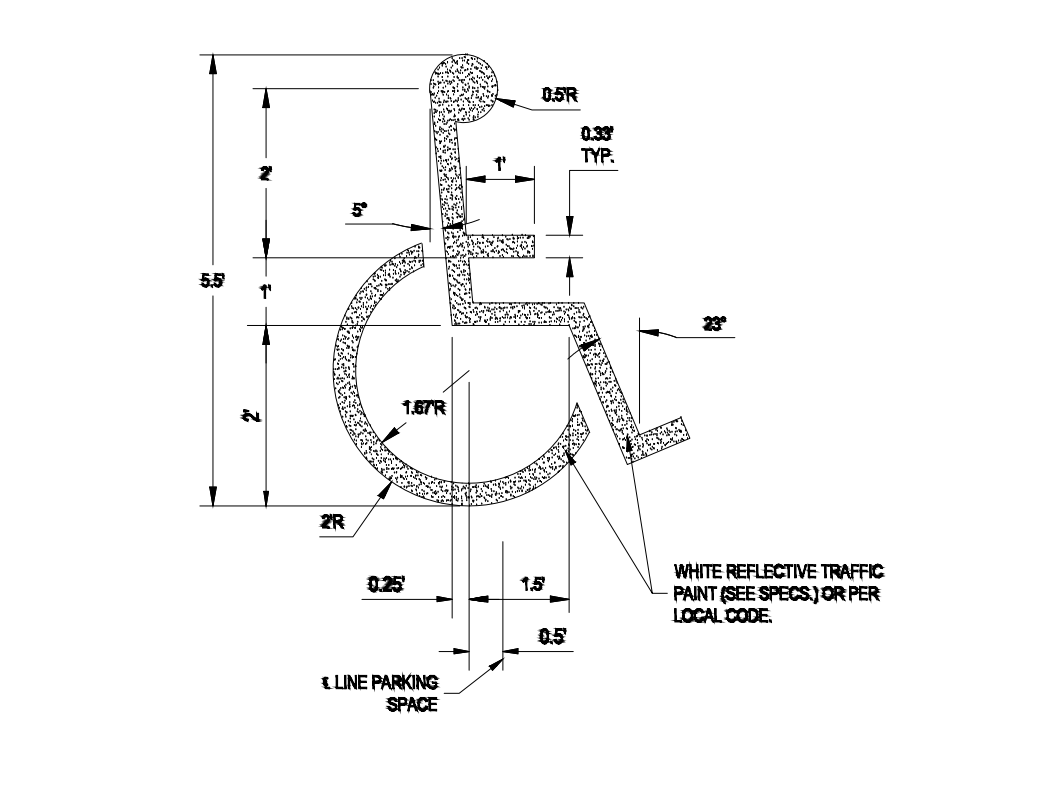
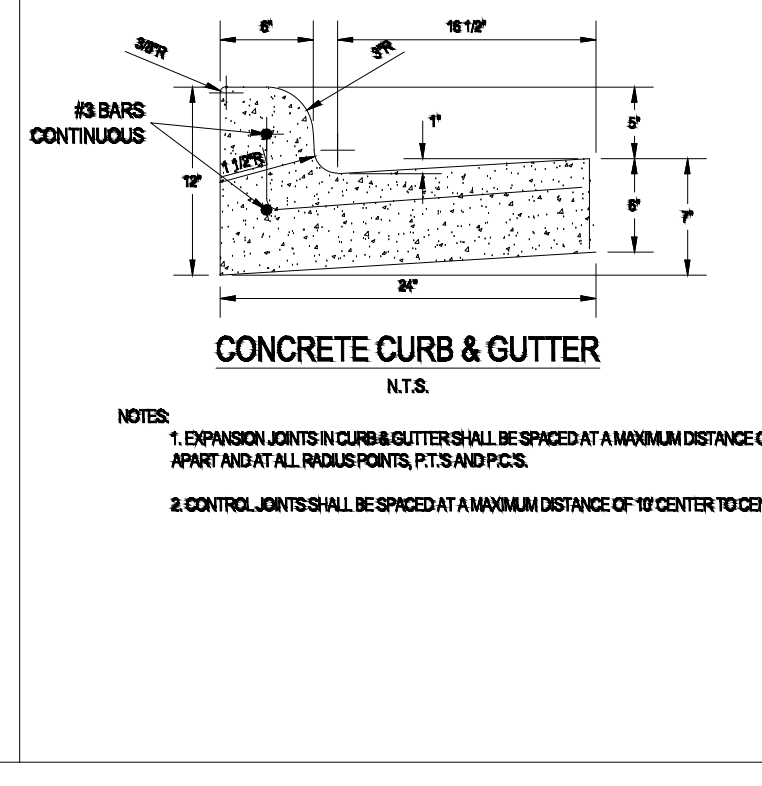
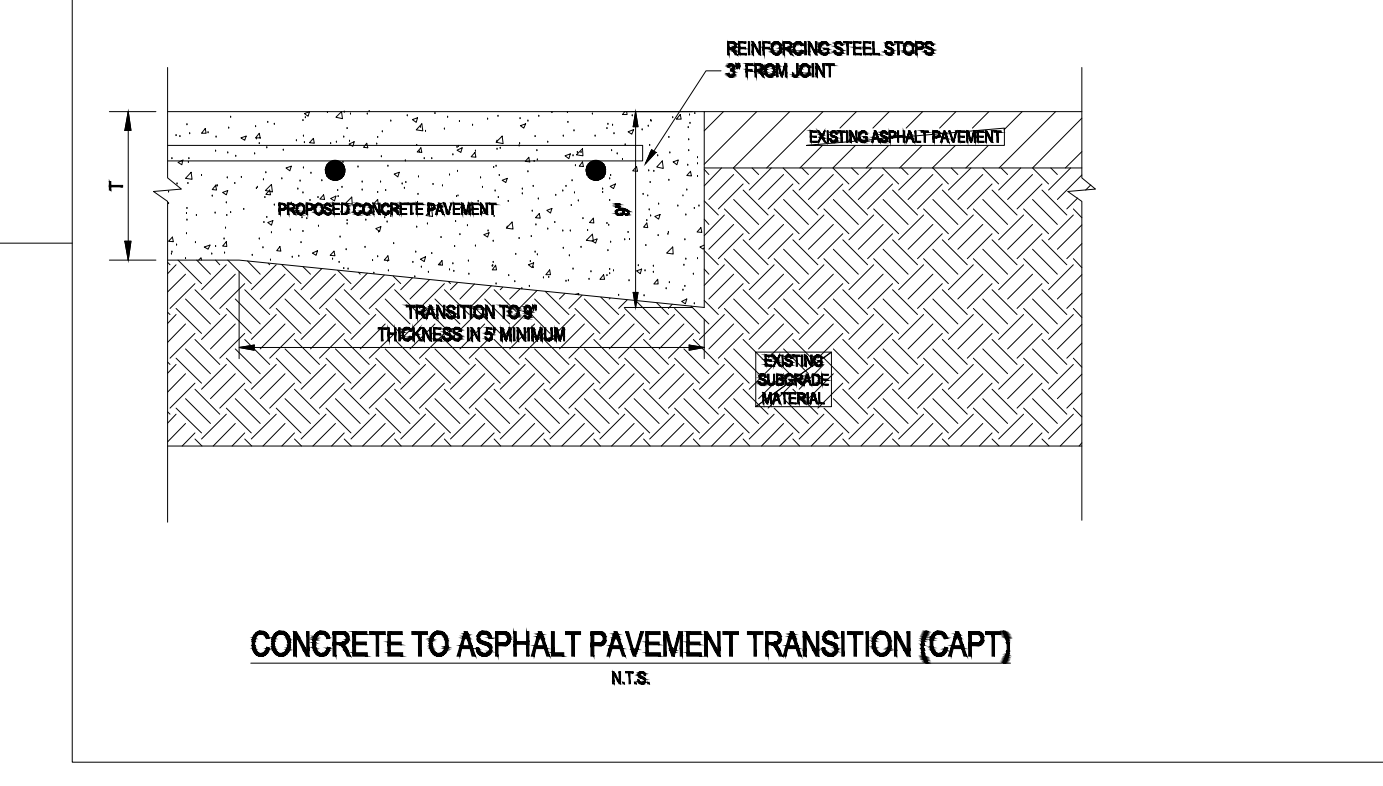
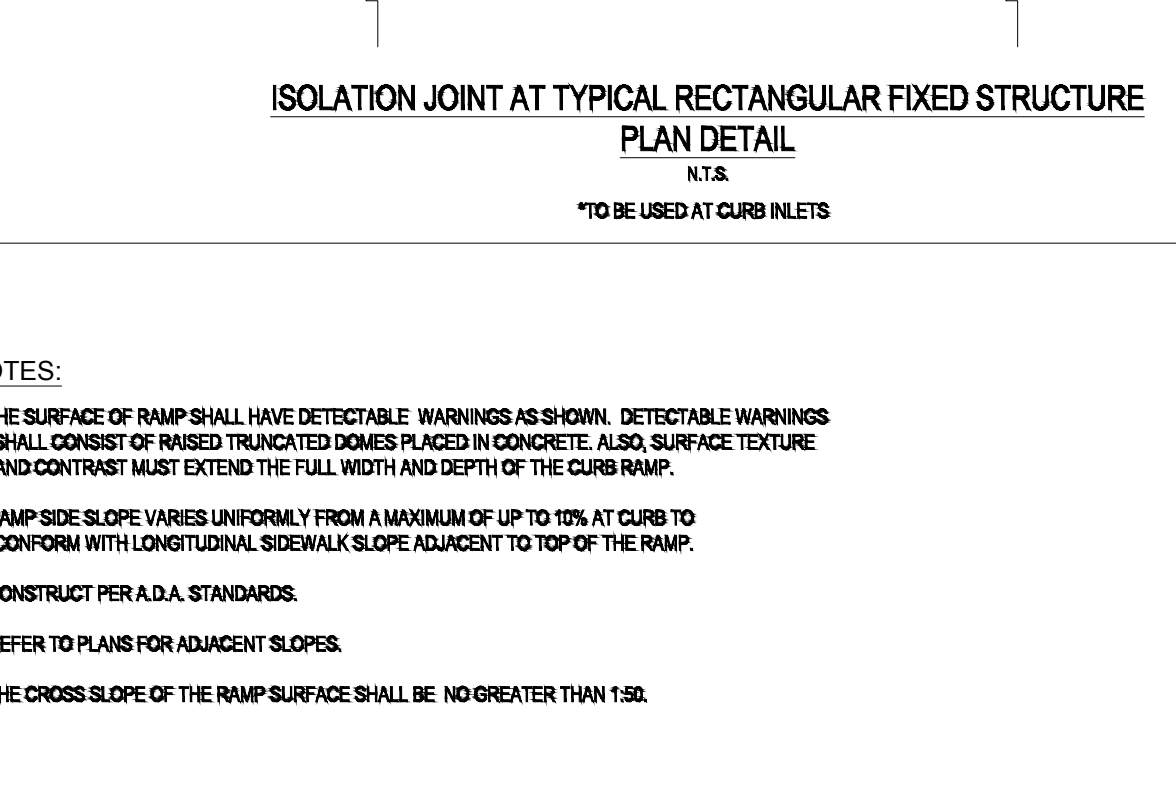
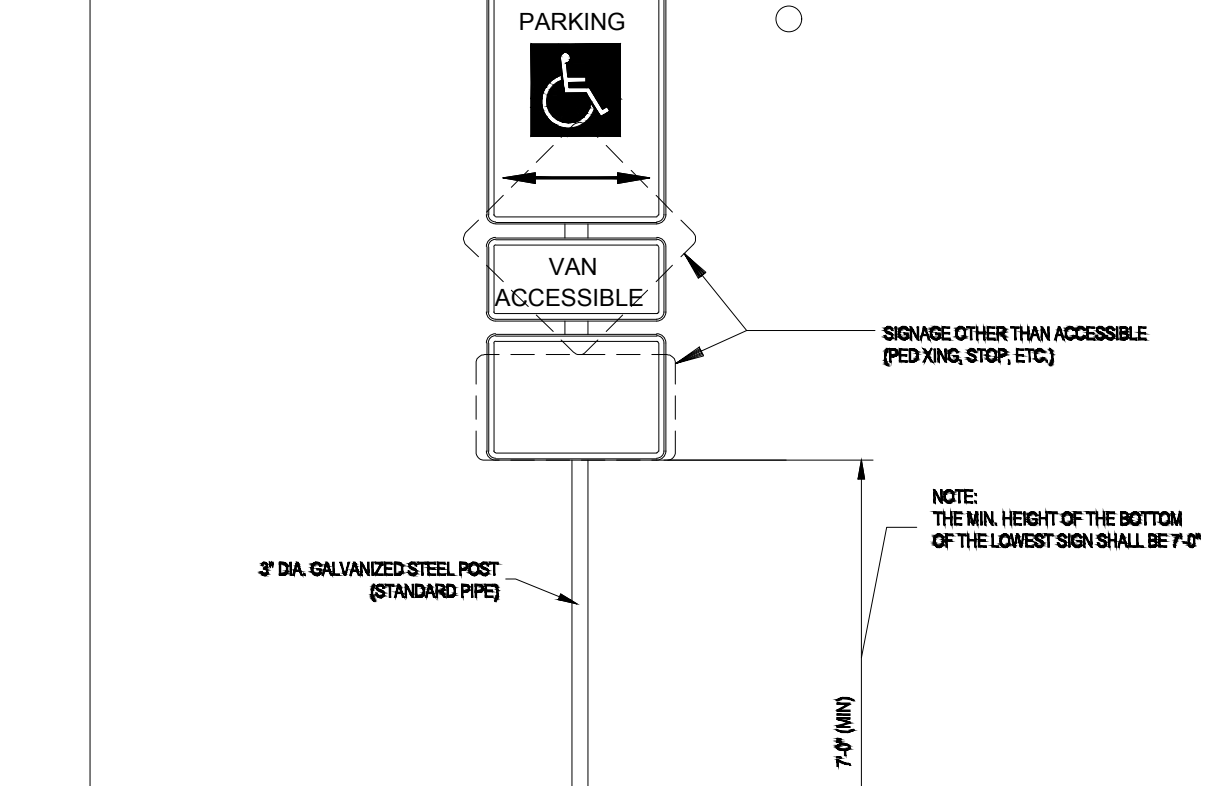
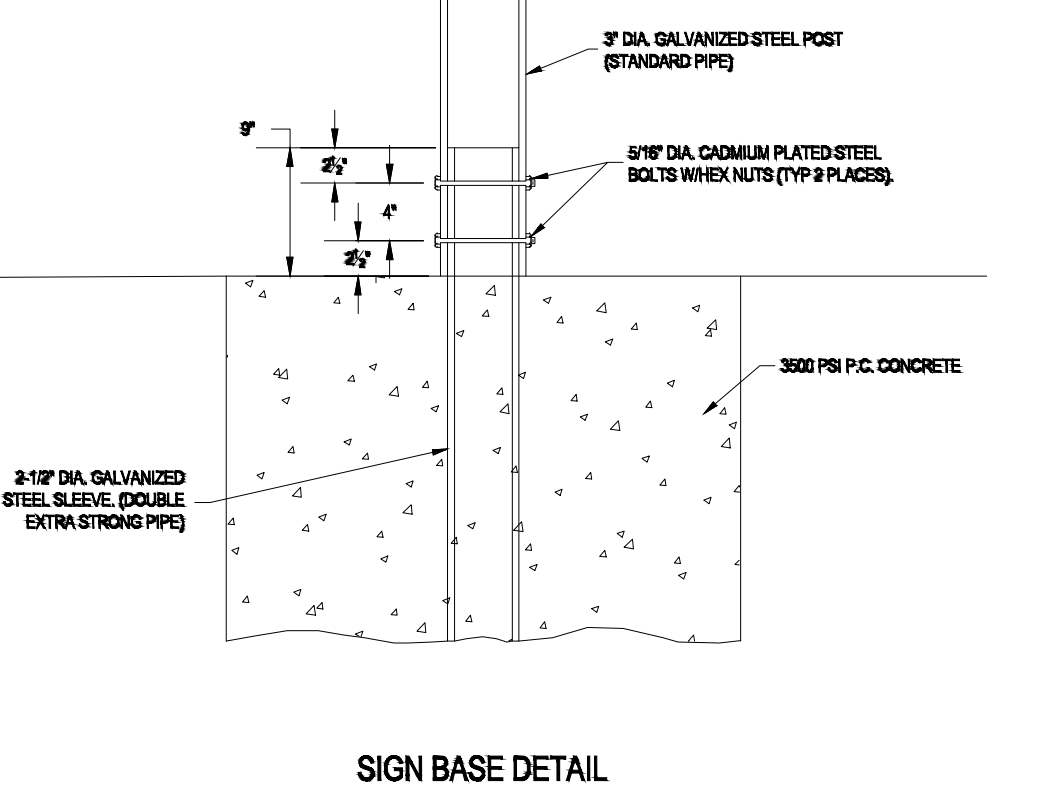
THE JOINT FILLER FOR ISOLATION JOINTS SHALL EXTEND THROUGHOUT THE SLAB THICKNESS TO THE SUBGRADE AND SHALL BE RECESSED BELOW THE PAVEMENT SURFACE SO THAT THE JOINT CAN BE SEALED WITH JOINT SEALANT MATERIALS.

RECOMMENDED JOINT FILLER MATERIALS INCLUDE: BITUMINOUS MASTIC, BITUMINOUS IMPREGNATED CELLULOSE OR CORK, SPONGE RUBBER, OR RESIN-BOUND CORK, AND SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.



**GENERAL PAVING NOTES**

1. SAND IS PROHIBITED FOR USE AS A LEVELING COURSE FOR FINAL FINISHING. THE GRADING SHALL BE PERFORMED WITH SELECT FILL. UPON COMPLETION OF FINAL GRADING, SUBGRADE SHALL BE LIGHTLY MOISTENED AS NEEDED, AND RECOMPACTED TO OBTAIN A TYPICAL SUBGRADE.
2. SUBGRADE MOISTURE CONTENT AND DENSITY MUST BE MAINTAINED UNTIL PAVING IS COMPLETED.
3. PROOF ROLLING OF SUBGRADE AND BASE SHALL BE PERFORMED IN ACCORDANCE WITH CITY OF LONGVIEW REQUIREMENTS.
4. FINAL PAVEMENT DESIGN SUBJECT TO APPROVAL BY CITY ENGINEER.



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**DRAWN BY:** BOULEVARD

**CHECKED:**

**APPROVED:**

**SHEET TITLE:**  
TYPICAL ACCESSIBLE DETAIL

**SHEET #:**  
C-3.0



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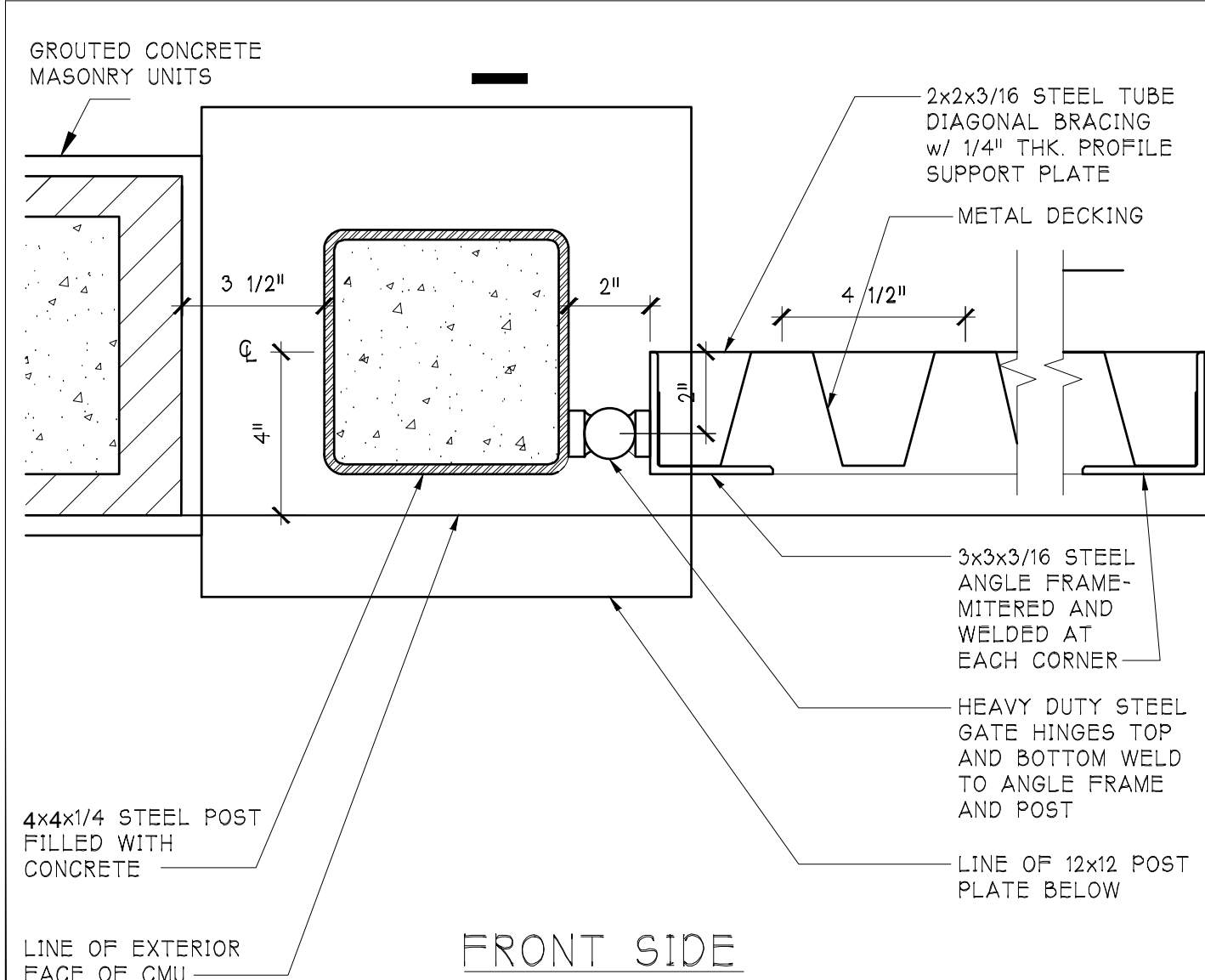
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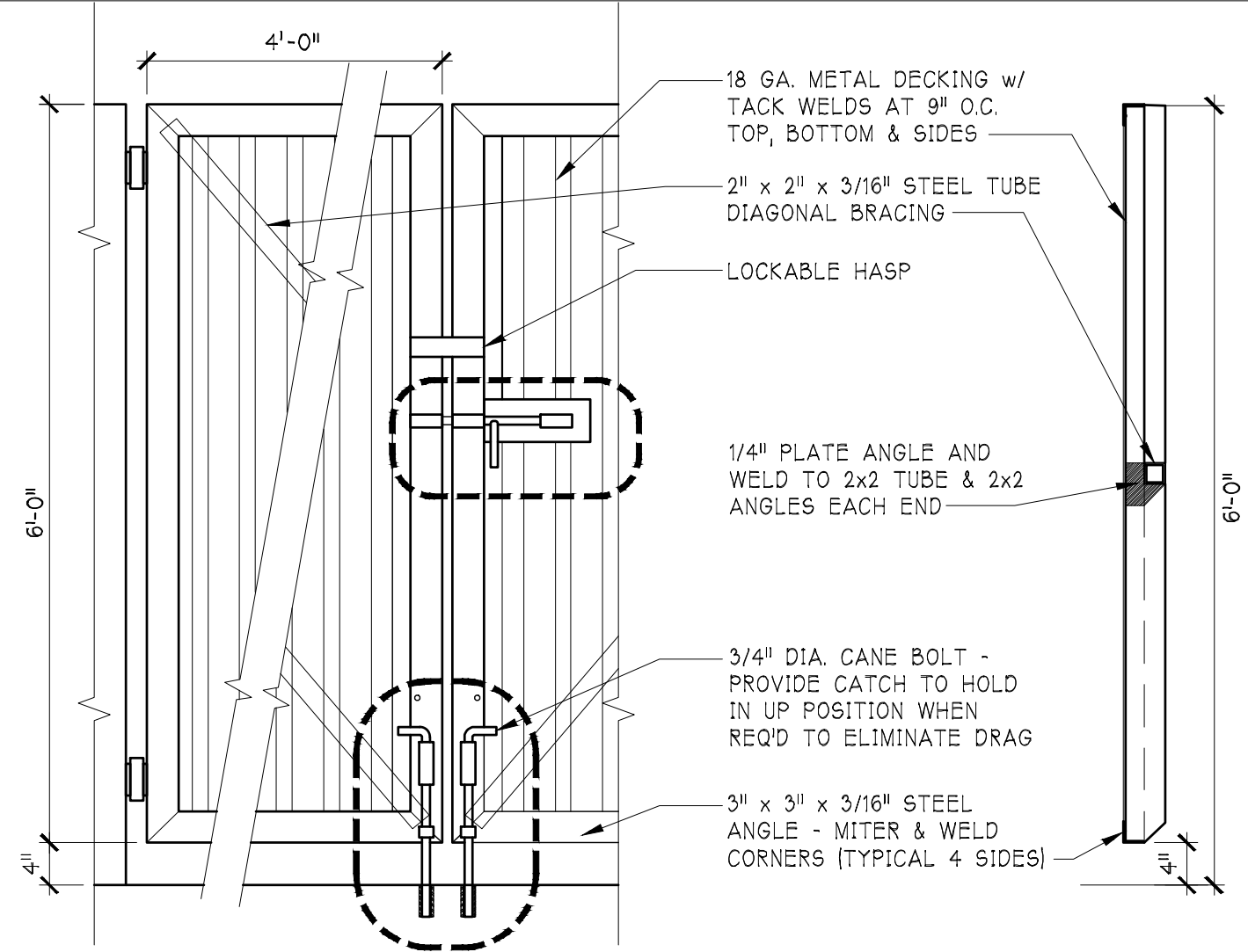
DRAWN BY: BOULEVARD  
 CHECKED:  
 APPROVED:

SHEET TITLE  
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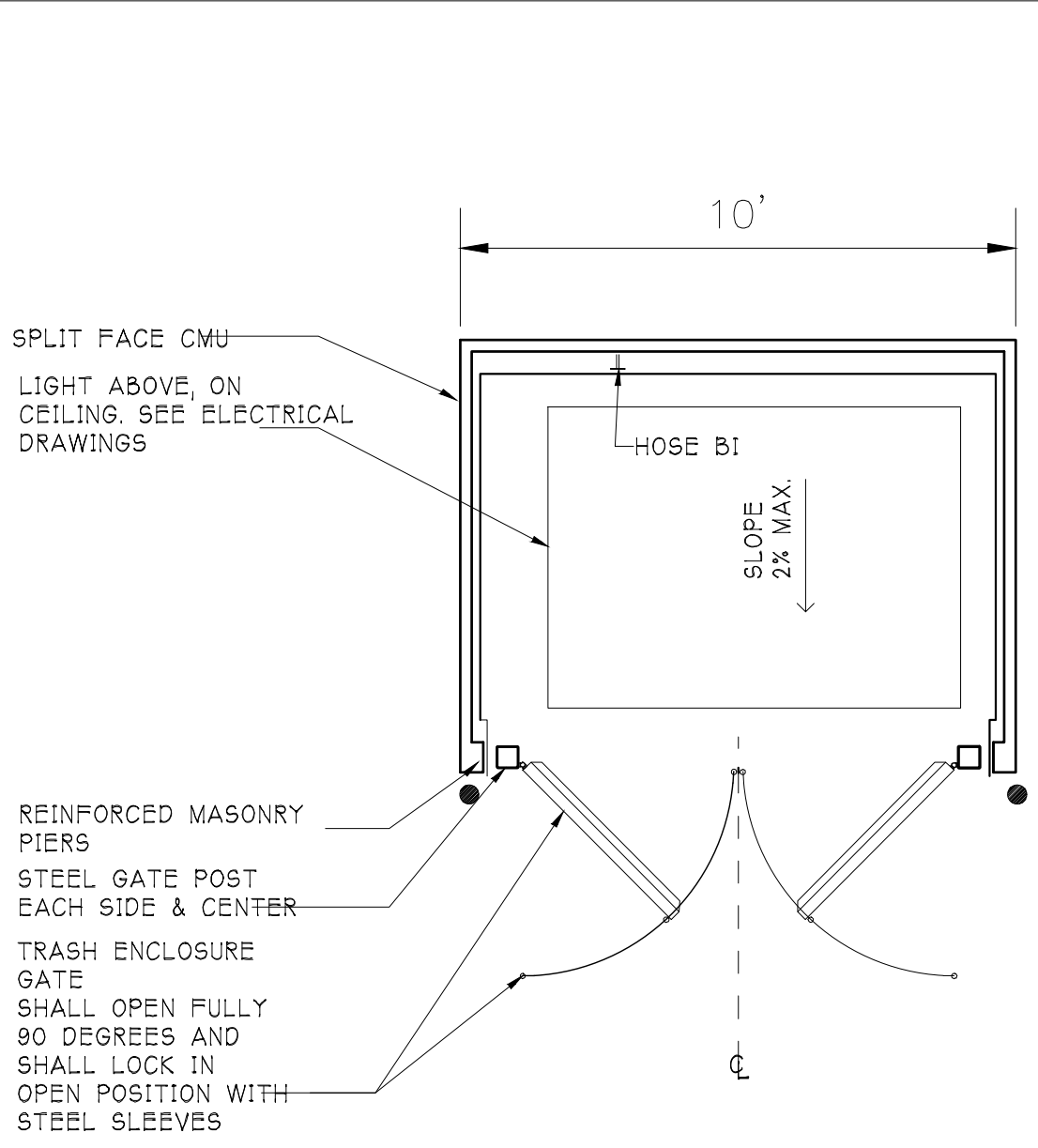
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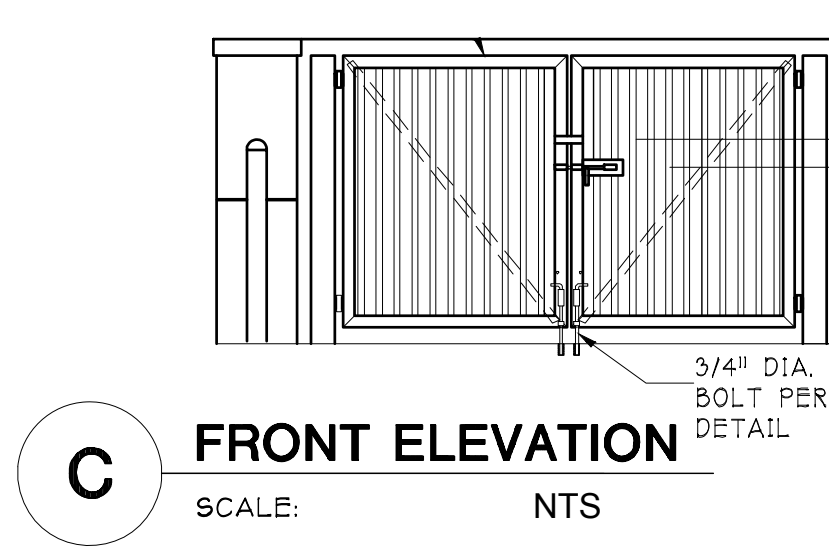
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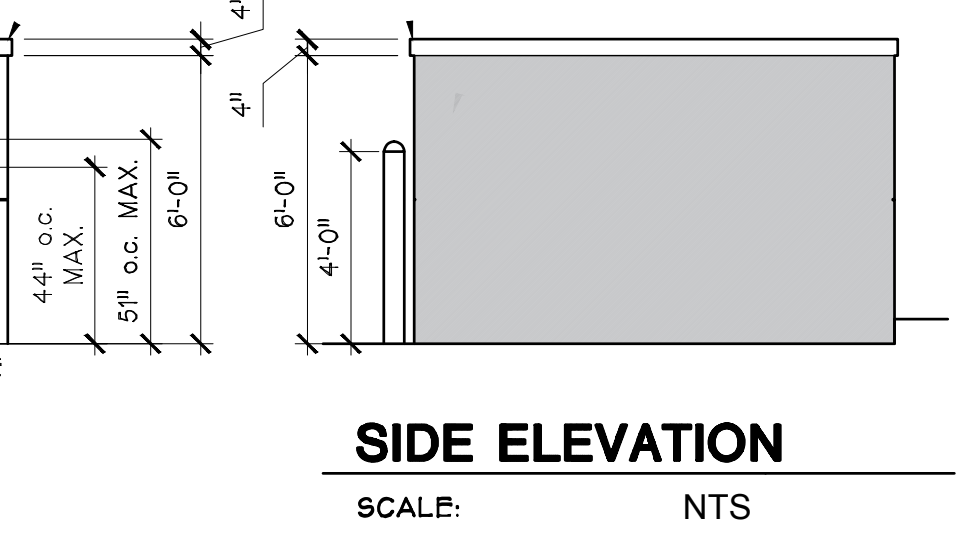
TRASH ENCLOSURE GATE ELEVATION & SECTION  
 SCALE: NTS



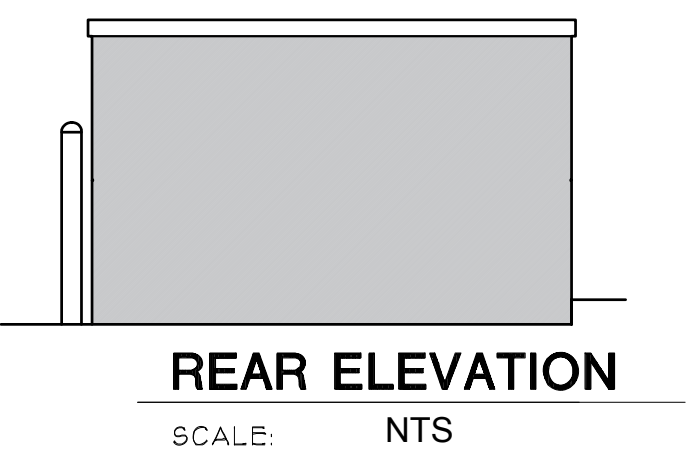
TRASH ENCLOSURE PLAN  
 SCALE: NTS



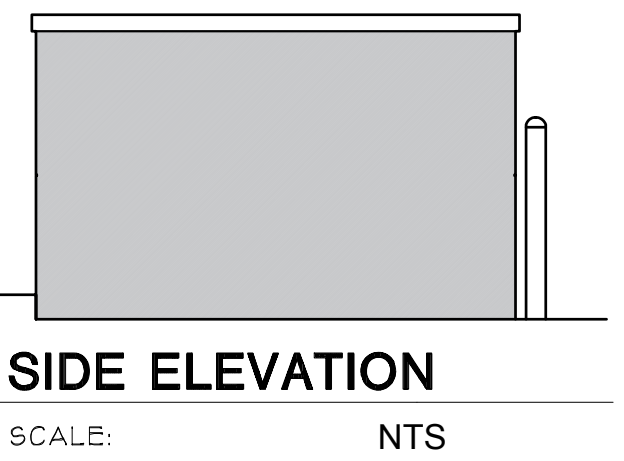
FRONT ELEVATION  
 SCALE: NTS



SIDE ELEVATION  
 SCALE: NTS

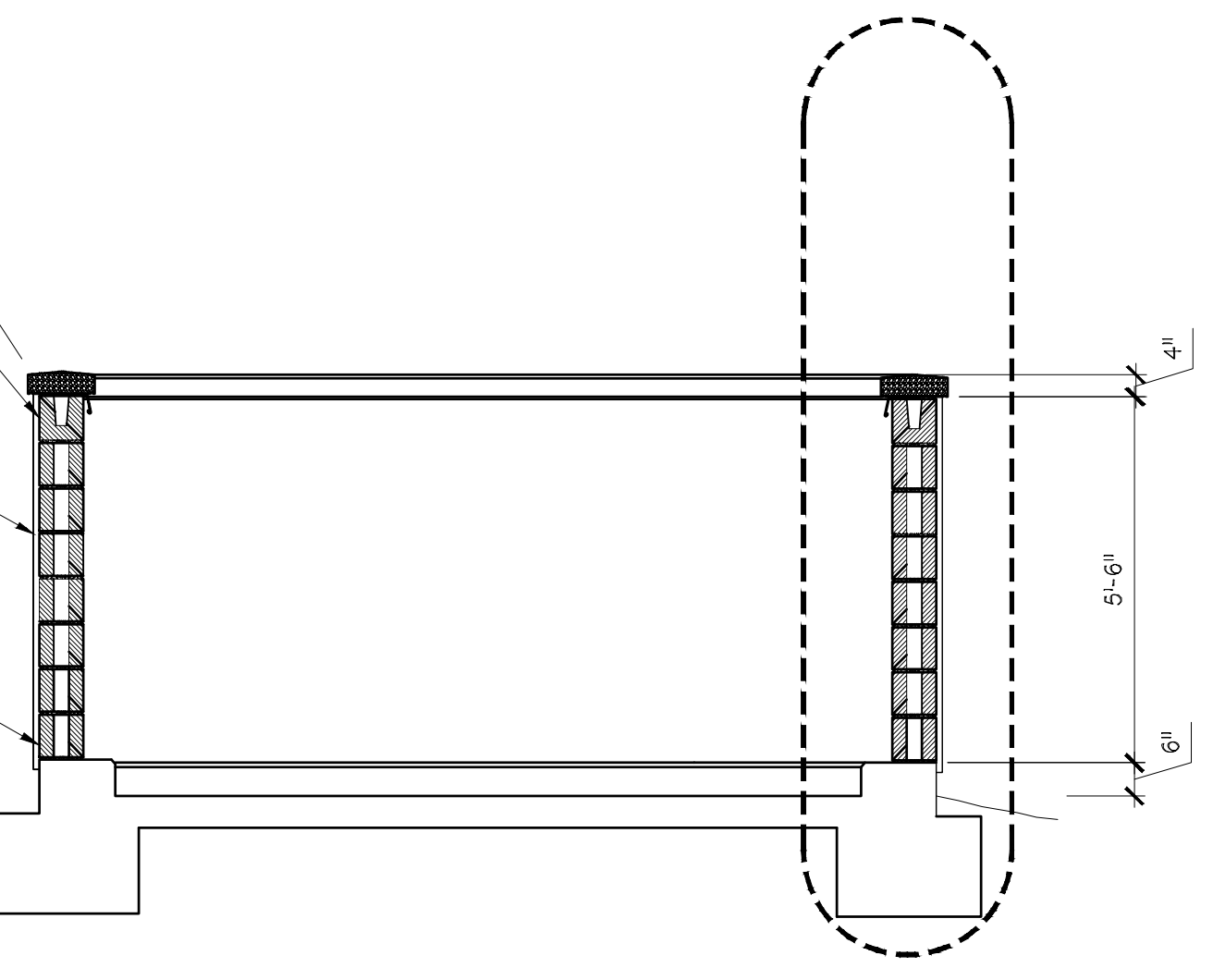


REAR ELEVATION  
 SCALE: NTS

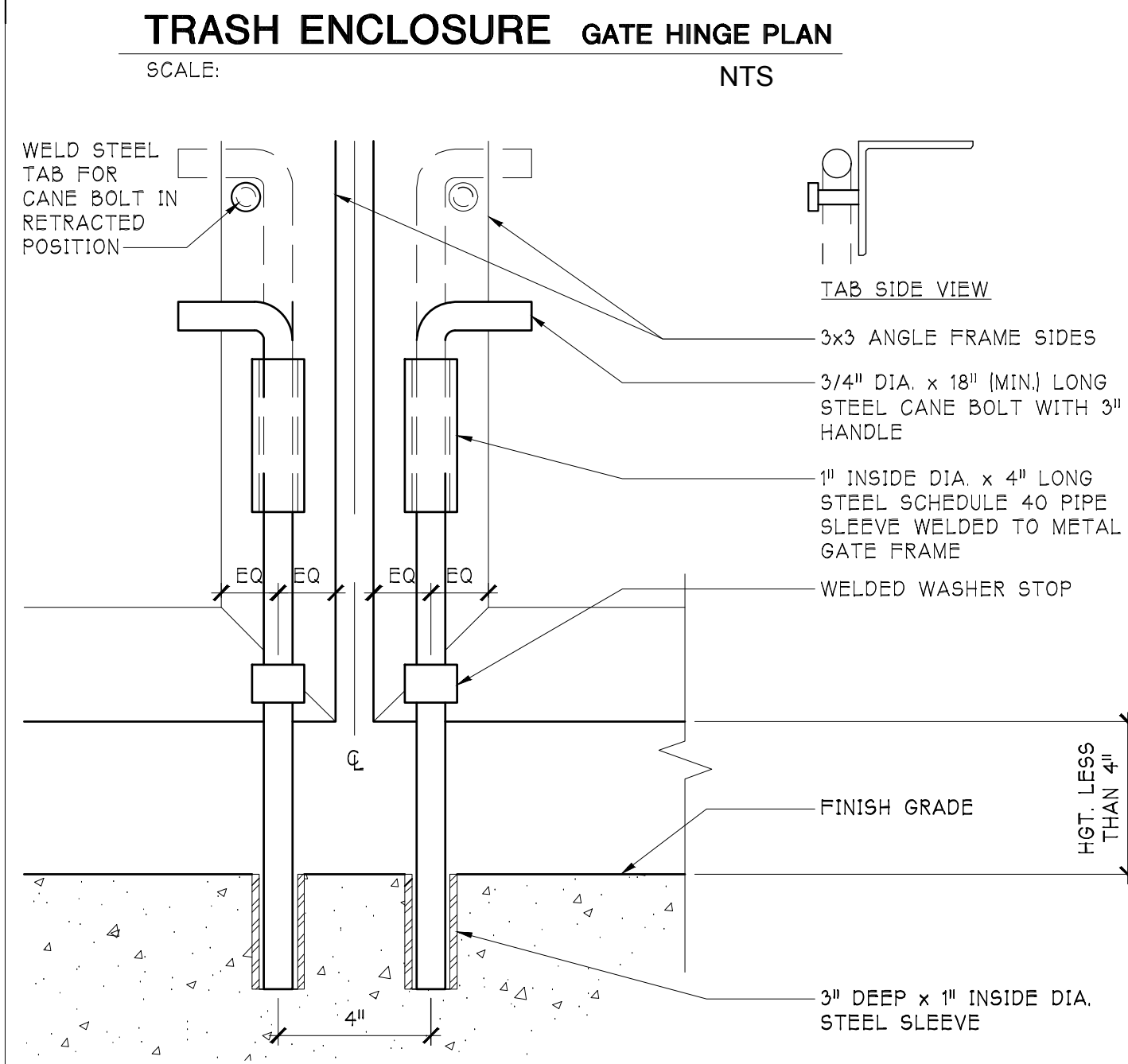


SIDE ELEVATION  
 SCALE: NTS

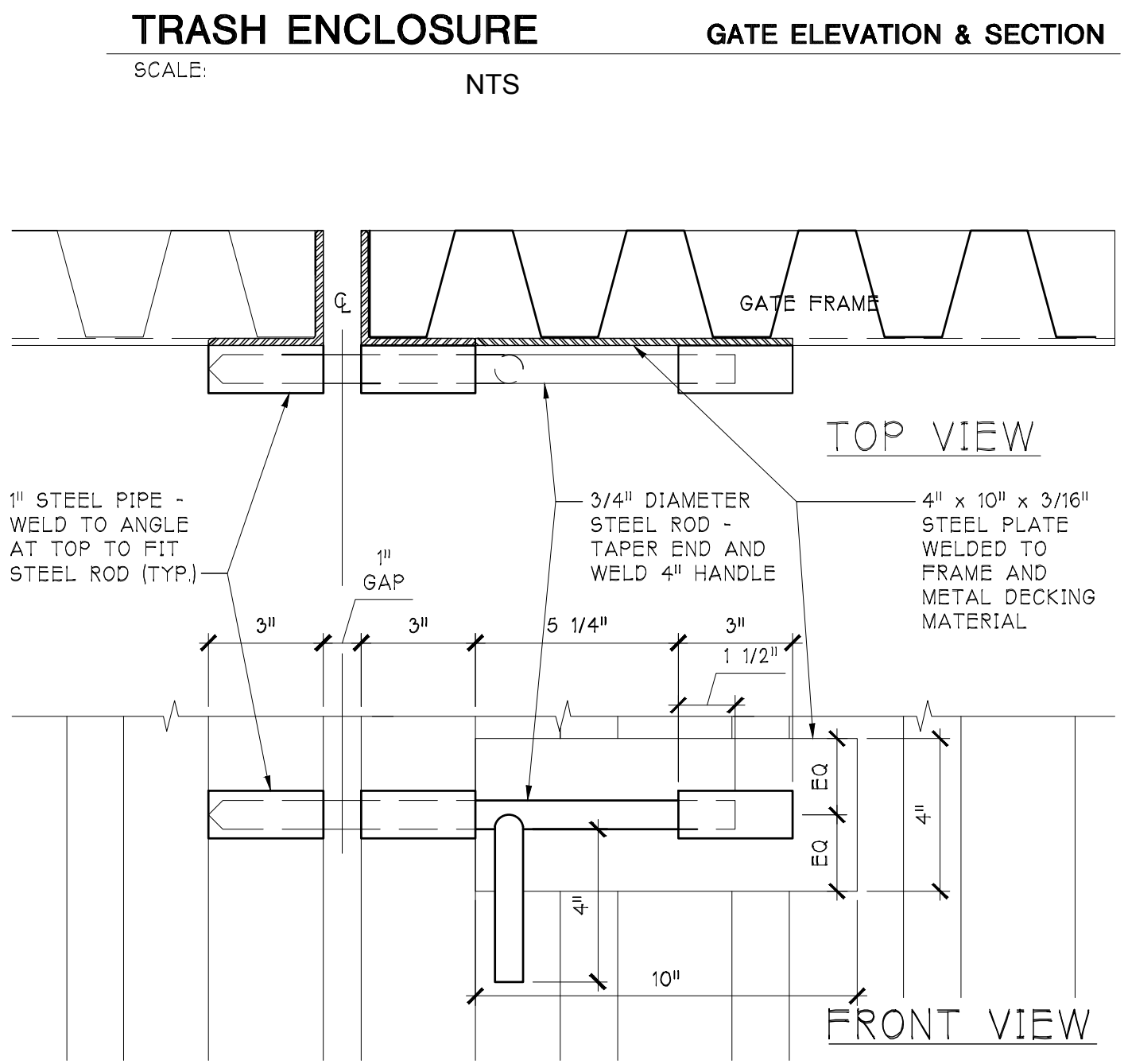
TRASH ENCLOSURE  
 SCALE: NTS



TRASH ENCLOSURE C.M.U. SECTION  
 SCALE: NTS



TRASH ENCLOSURE GATE HINGE PLAN  
 SCALE: NTS



TRASH ENCLOSURE GATE ELEVATION & SECTION  
 SCALE: NTS



NEUTRAL WHEAT 10YY  
 35/094  
 CARBON COPY BEHR PPU25-01 F2  
 PAINT ON STACCO  
 USAGE: INTERIOR & EXTERIOR SURFACES



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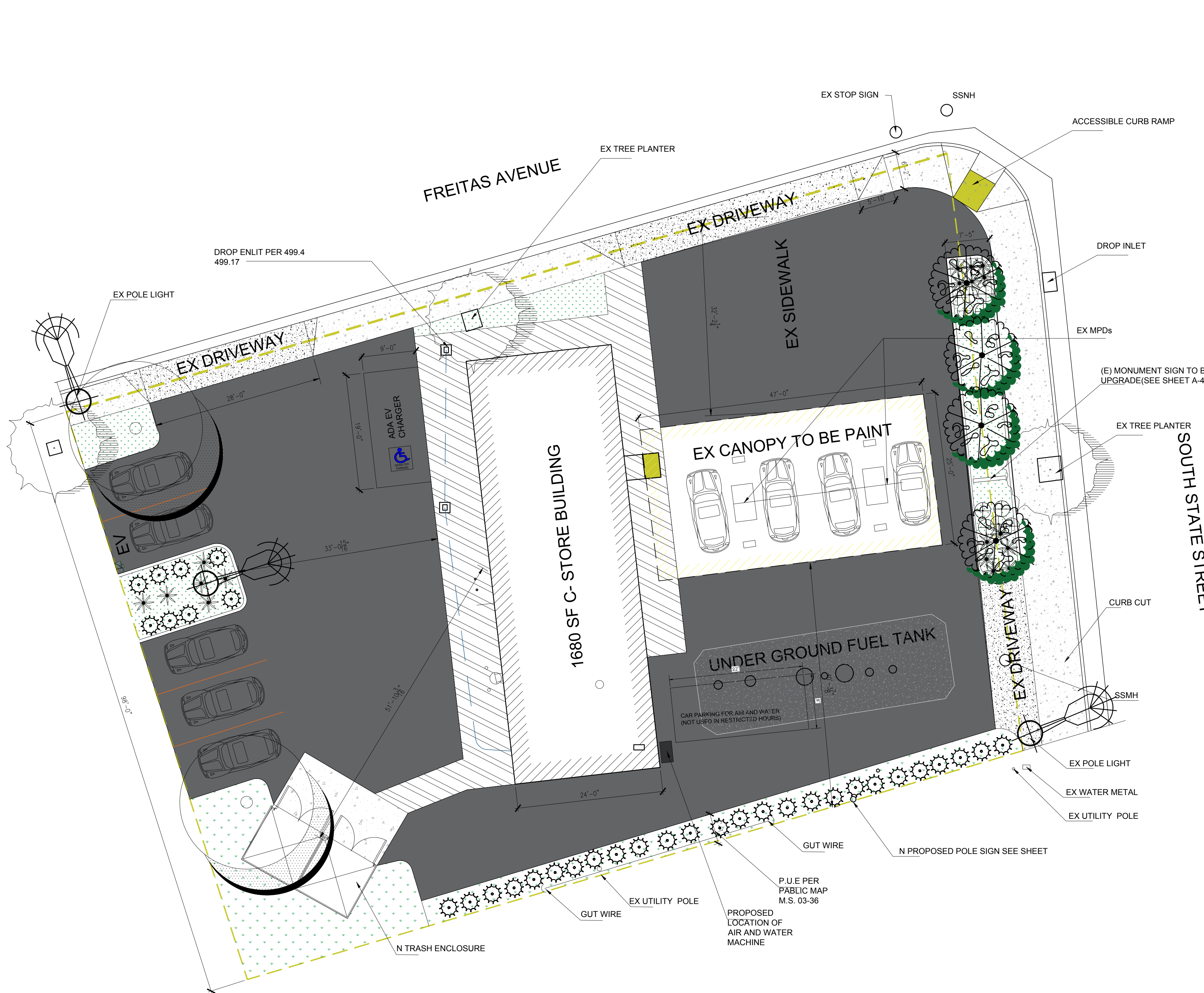
**ISSUED FOR:**  
 01,24,2024

**CONTROL NO.:**

**DRAWN BY:** BOULEVARD  
**CHECKED:**  
**APPROVED:**

**SHEET TITLE:**  
 PROPOSED LANDSCAPE PLAN

**SHEET #**  
 L-1.0



**PARKING LOT SHADE TREE** 2 24" box  
 Pistacia chinensis 'Keith Davey' / Keith Davey Chinese Pistache  
 Ulmus parvifolia 'Drake' / Drake Lacebark Elm

**STREET TREE** 4 Low 10-30%  
 Chitalpa tashkentensis  
 Chilopsis linearis

**SMALL ACCENT SHRUBS** 18  
 Small shrub with interesting texture or flowers, for mass planting and borders at high visibility locations and entries  
 Dianella revoluta 'DR5000' TM / Little Rev Flax Lily 1 gal.  
 Dianella revoluta 'Variegated' / Variegated Flax Lily 1 gal.  
 Liriope muscari 'Majestic' / Majestic Lilyturf 1 gal.  
 Liriope muscari 'Silvery Sunproof' / Silvery Sunproof Lilyturf 1 gal.  
 Salvia microphylla / Graham Sage 1 gal.  
 Zauschneria californica / California Fuchsia 1 gal.

**INFILL SHRUB** 10  
 Evergreen shrub, 30-42" high, for mid ground and mass planting.  
 Callistemon viminalis 'Little John' / Little John Weeping Bottlebrush 5 gal.  
 Cistus x purpureus / Orchid Rockrose 5 gal.  
 Diets indioides / Fortnight Lily 5 gal.  
 Nandina domestica 'Gulf Stream' TM / Gulf Stream Heavenly Bamboo 5 gal.

**EVERGREEN HEDGE** 34  
 Small to medium hedge, 30"-42" high, may be informal or formal (sheared) hedge, depending on space and location  
 Ligustrum japonicum 'Texanum' / Texanum Privet 5 gal.  
 Olea europaea 'Little Ollie' TM / Little Ollie Olive 5 gal.  
 Rhampholepis umbellata 'Minor' / Yedda Hawthorn 5 gal.

**EXISTING TREES TO BE REMAIN**

**LANDSCAPE CALCULATIONS**

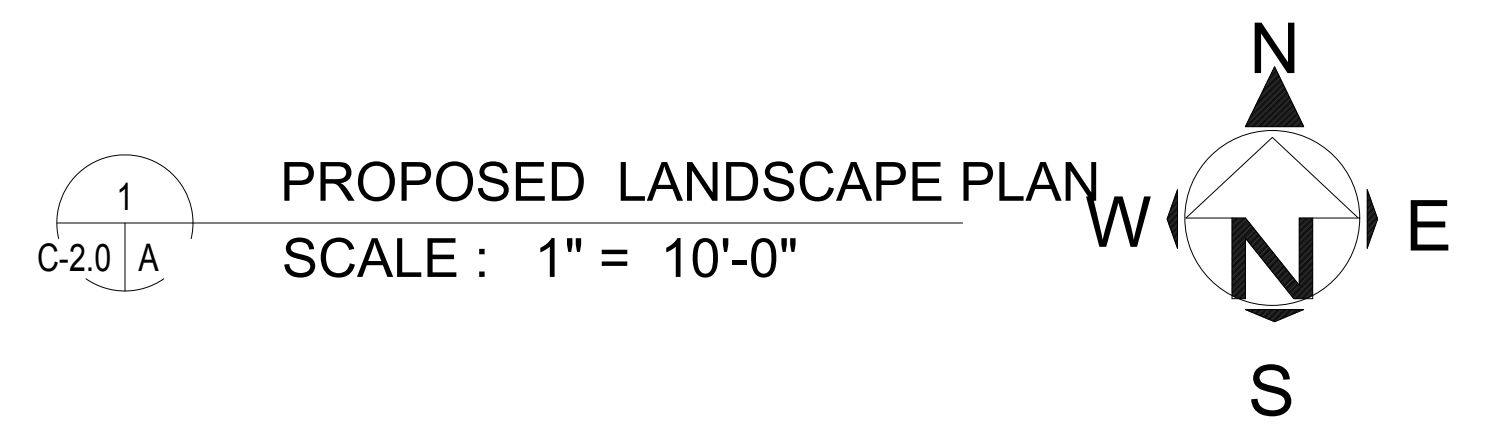
<b>LANDSCAPE AREA</b>	
Total Site Area:	13,916 sf.
Total Landscape Area:	1,740 sf.
Percentage of Total Site as Landscape:	12.5 %

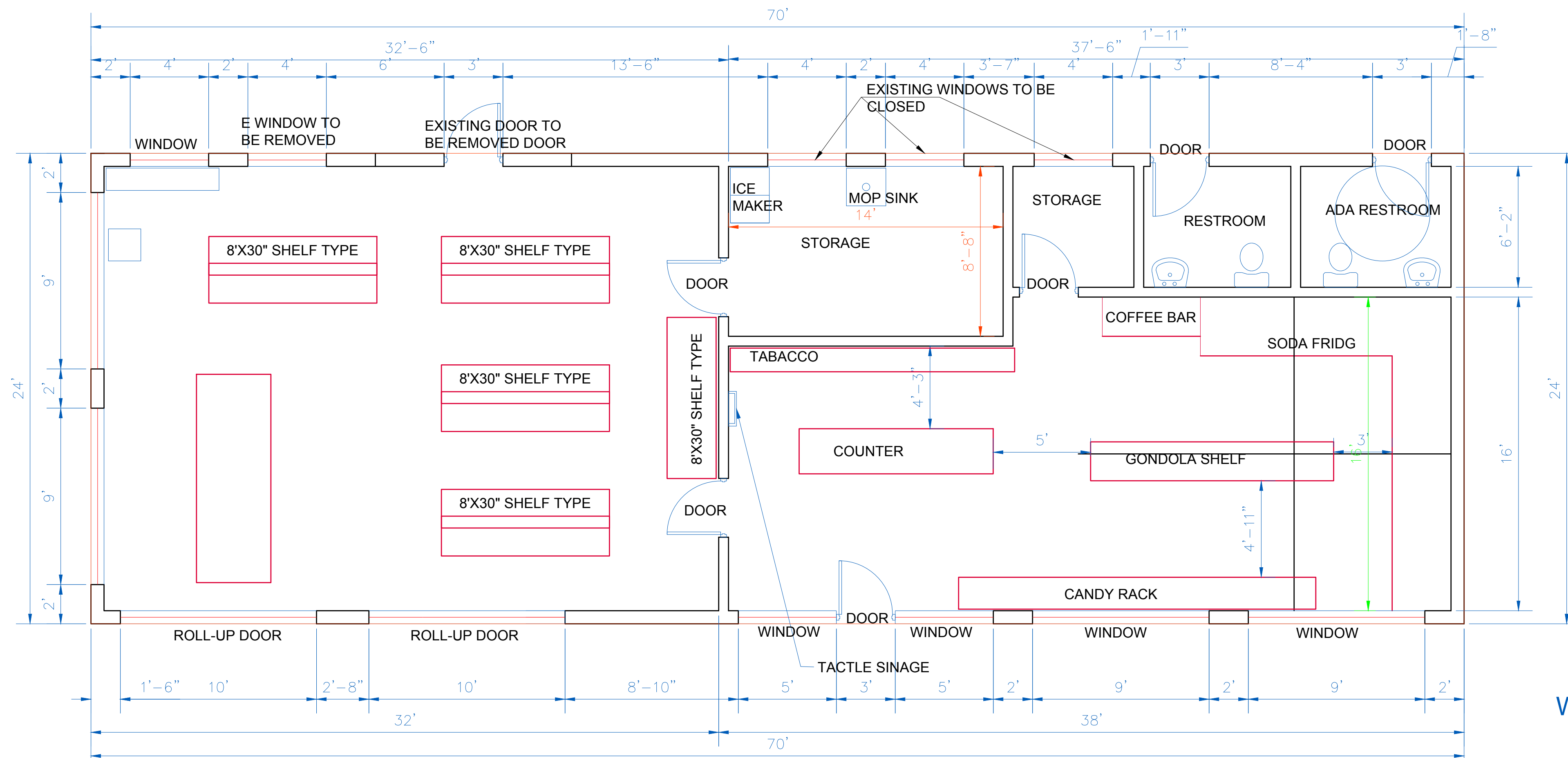
**PARKING LOT SHADE CALCULATIONS**

Tree diameters per species based on published County documentation or the Sunset Western Garden Book. Shade area on plans is identified with the hatch symbol to the left.

Tree Type	Quantity					Subtotal (sf)
	Area at 100%	100%	75%	50%	25%	
26' diameter Parking lot shade tree	707 sf	0	0	354(2)	0	708 SF

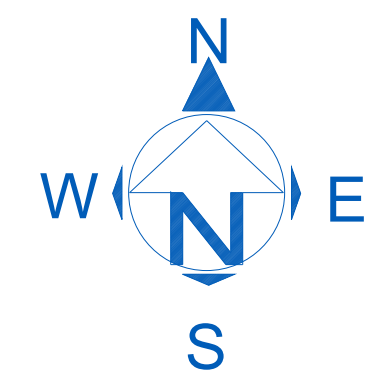
Total shade provided: 708  
 Parking area: 1026  
 Percentage of shade provided (min. 50% req'd): 69 %



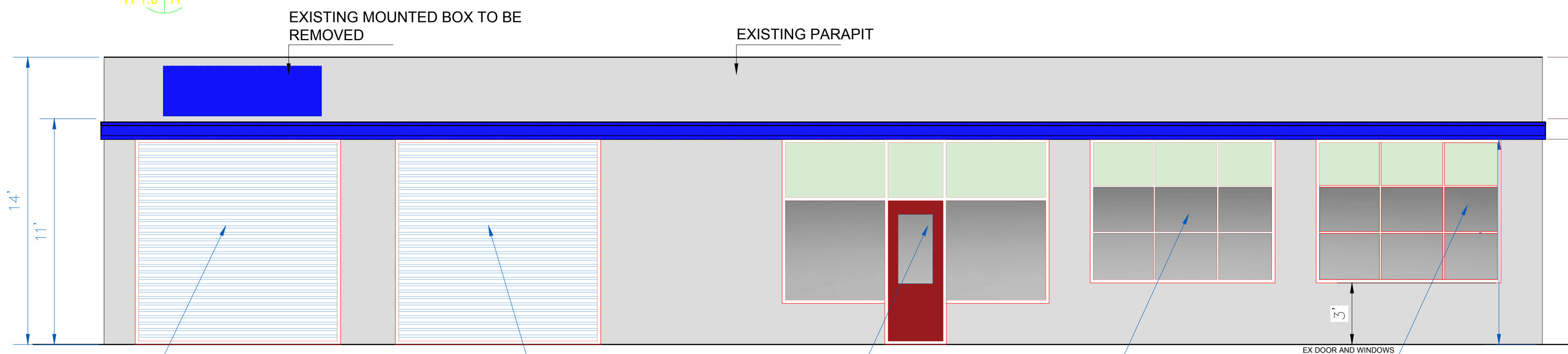


**EXISTING FLOOR PLAN**

SCALE: 1/4" = 1'-0"

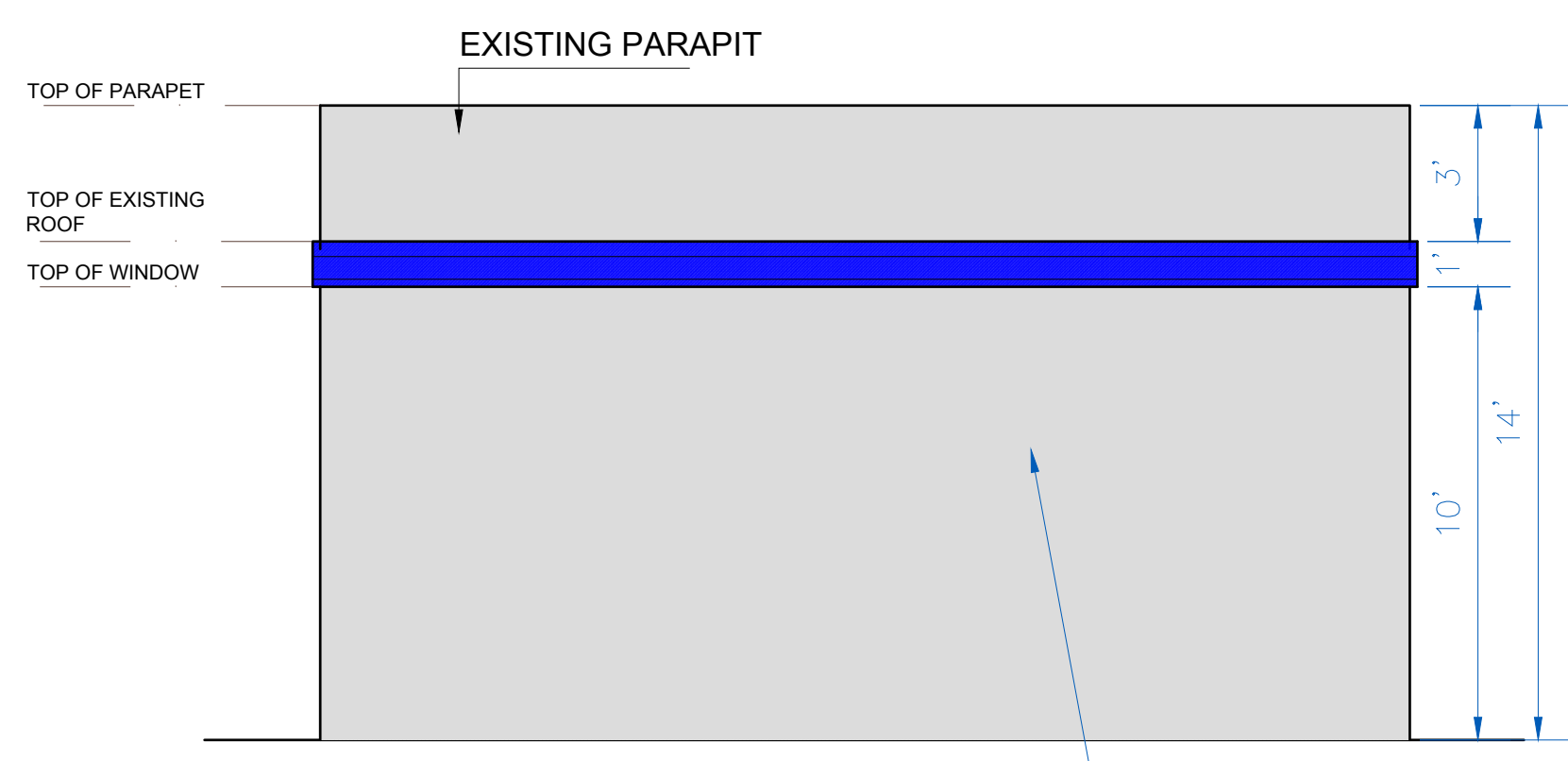


**EX BUILDING PICTURES**



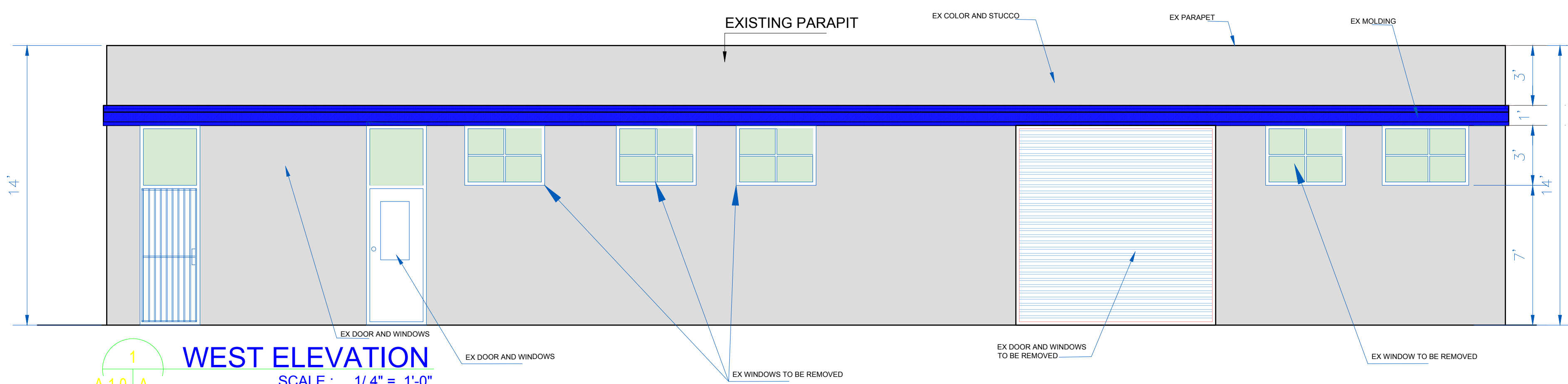
**EAST ELEVATION**

SCALE: 1/4" = 1'-0"



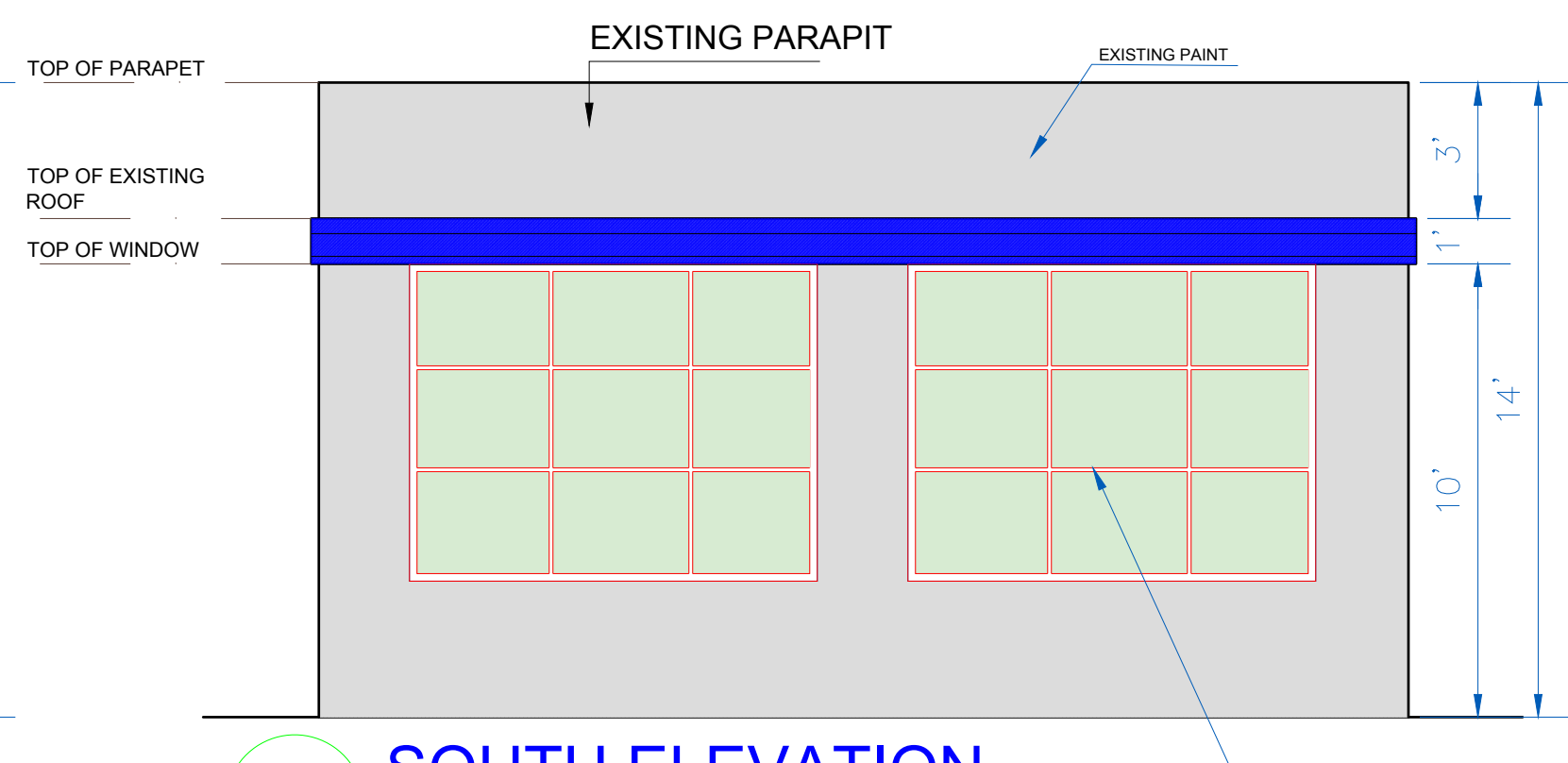
**NORTH ELEVATION**

SCALE: 1/4" = 1'-0"



**WEST ELEVATION**

SCALE: 1/4" = 1'-0"



**SOUTH ELEVATION**

SCALE: 1/4" = 1'-0"



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**ARCHITECT OF RECORD:**  
**BOULEVARD**  
 Construction  
 4080 TRUXEL ROAD, SUITE 100,  
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 Website: https://theboulevard.us  
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 Email: info@theboulevard.us

**PROJECT:**  
 734 SOUTH STATE STREET UKIAH,  
 CA 95482

REVISIONS:	DATE:	DESCRIPTION:	BY:

**CURRENT ISSUE DATE:**  
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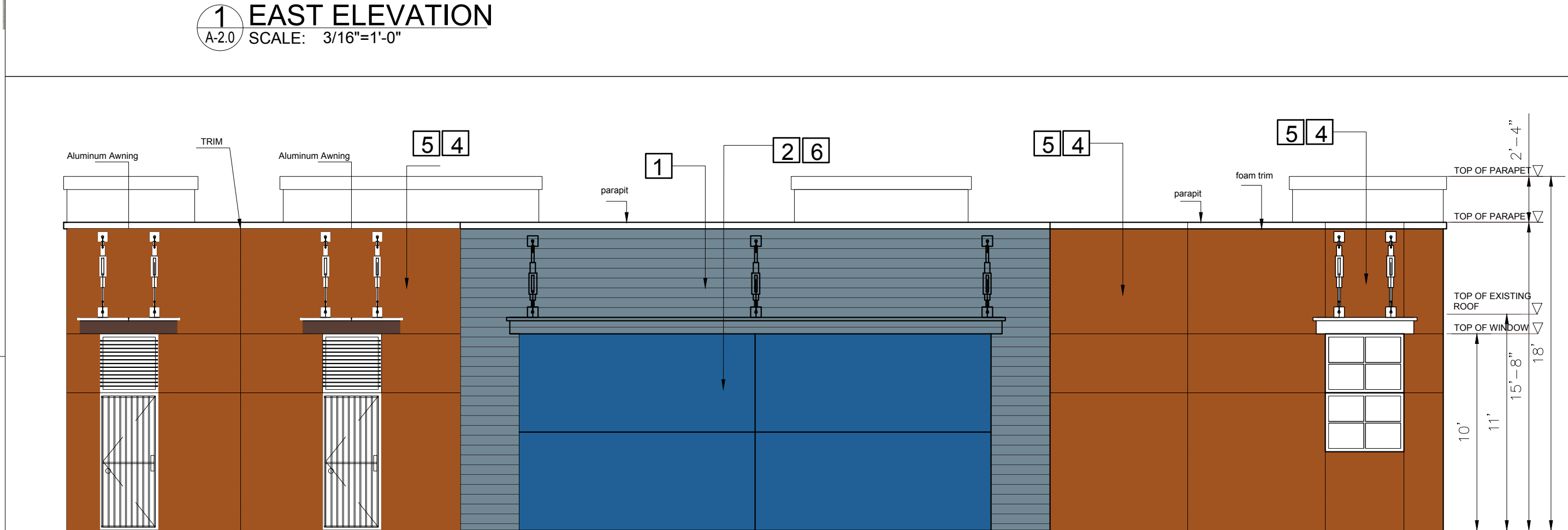
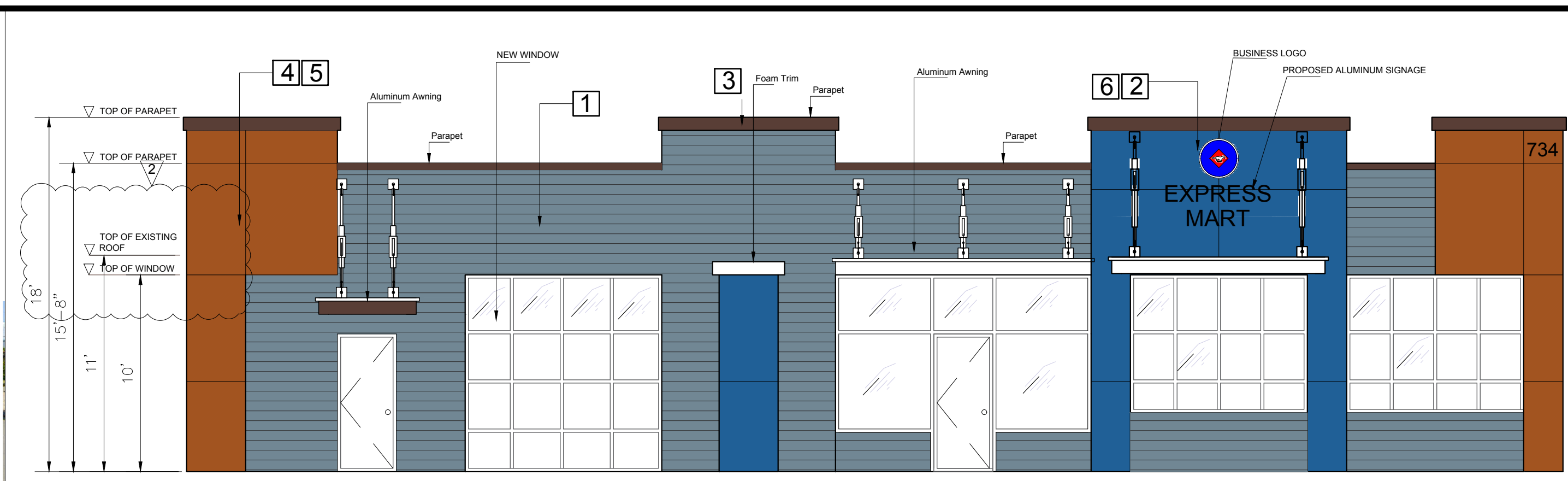
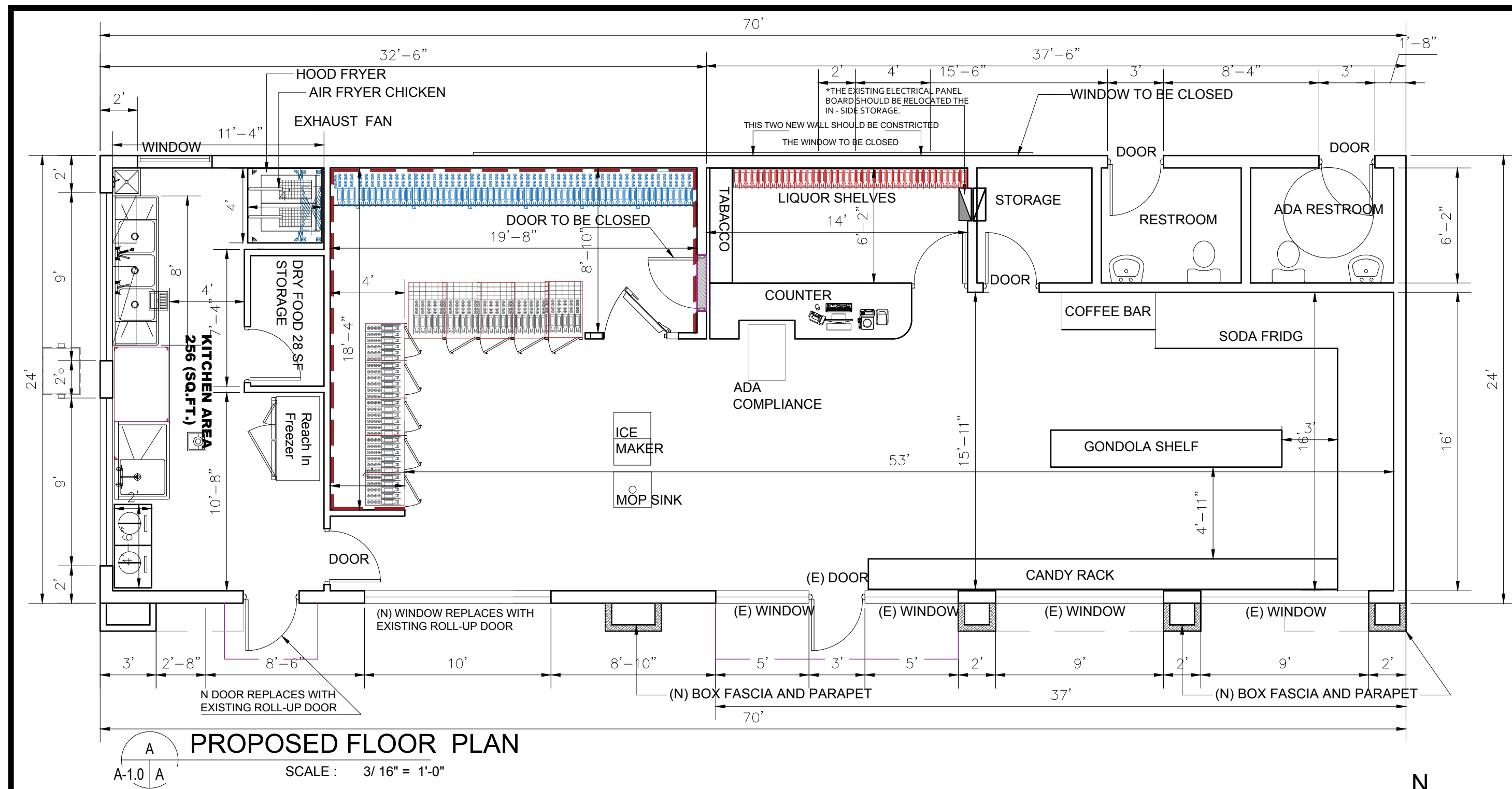
**ISSUED FOR:**  
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**CONTROL NO.:**

**DRAWN BY:** BOULEVARD  
**CHECKED:**  
**APPROVED:**

**SHEET TITLE:**  
 EXISTING PLAN AND ELEVATIONS

**SHEET #**  
 A-1.0



**1 FOR COLOR LAP SIDING**  
Fiber Cement Cedar Mill Siding 7.25"x144" Evening Blue 1pc

**2 FOR COLOR**  
#PPU15-03 Dark Cobalt Blue Extra Durable Flat Interior Paint & Primer

**3 FOR COLOR**  
Behr Dark Walnut (PPF-51) HEX code

**4 FOR COLOR**  
#S-H-260 Tiger Stripe Satin Enamel Interior Stain-Blocking Paint and Primer

**5 FOR STUCCO**  
Free high resolution close up photo of a golden brown colored stucco wall. Stucco is a cement or plaster based wall covering that is very popular in buildings throughout the United States.

**6 FOR STUCCO**  
BLUE STONE WALL TEXTURE STUCCO

**Pitco SG18-S Specifications**

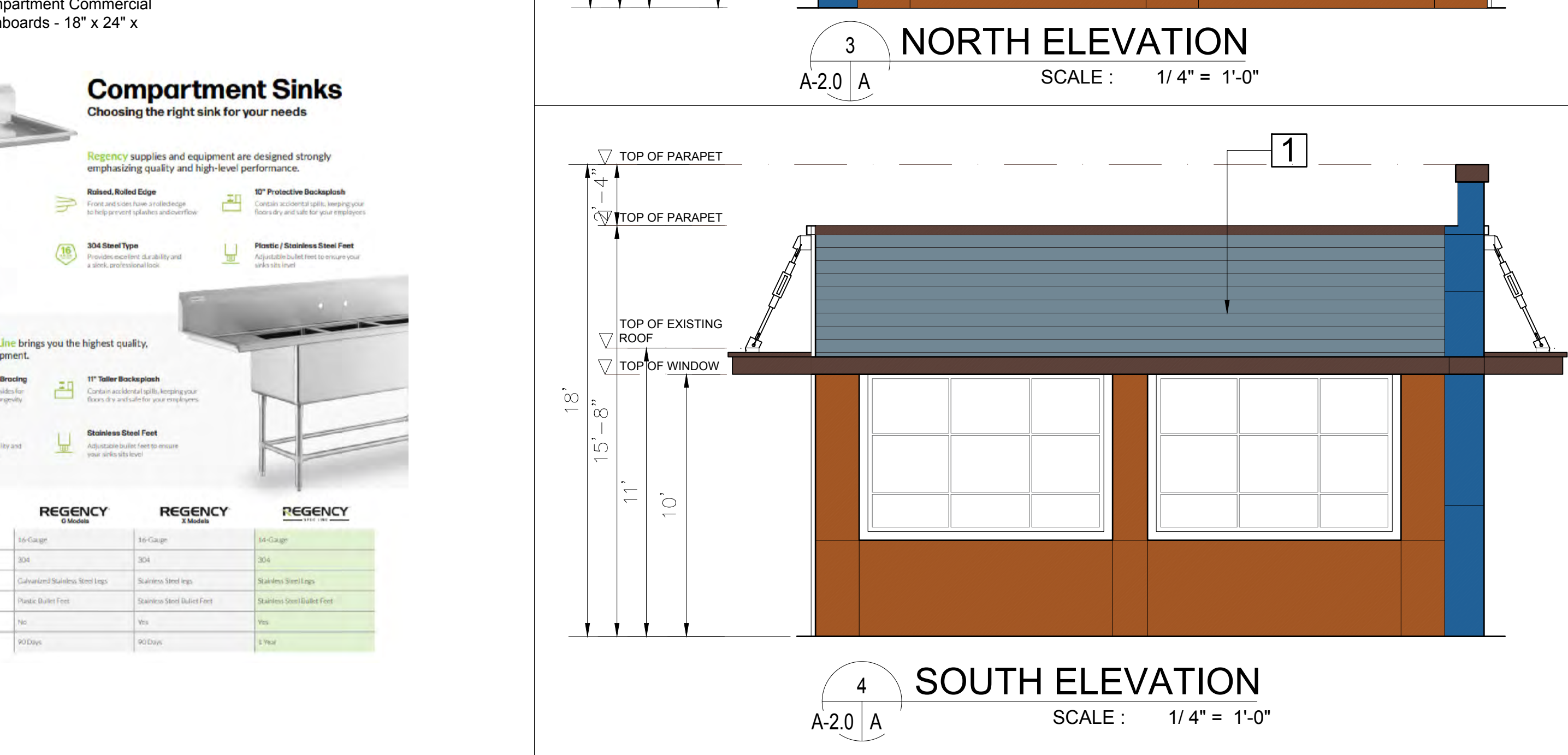
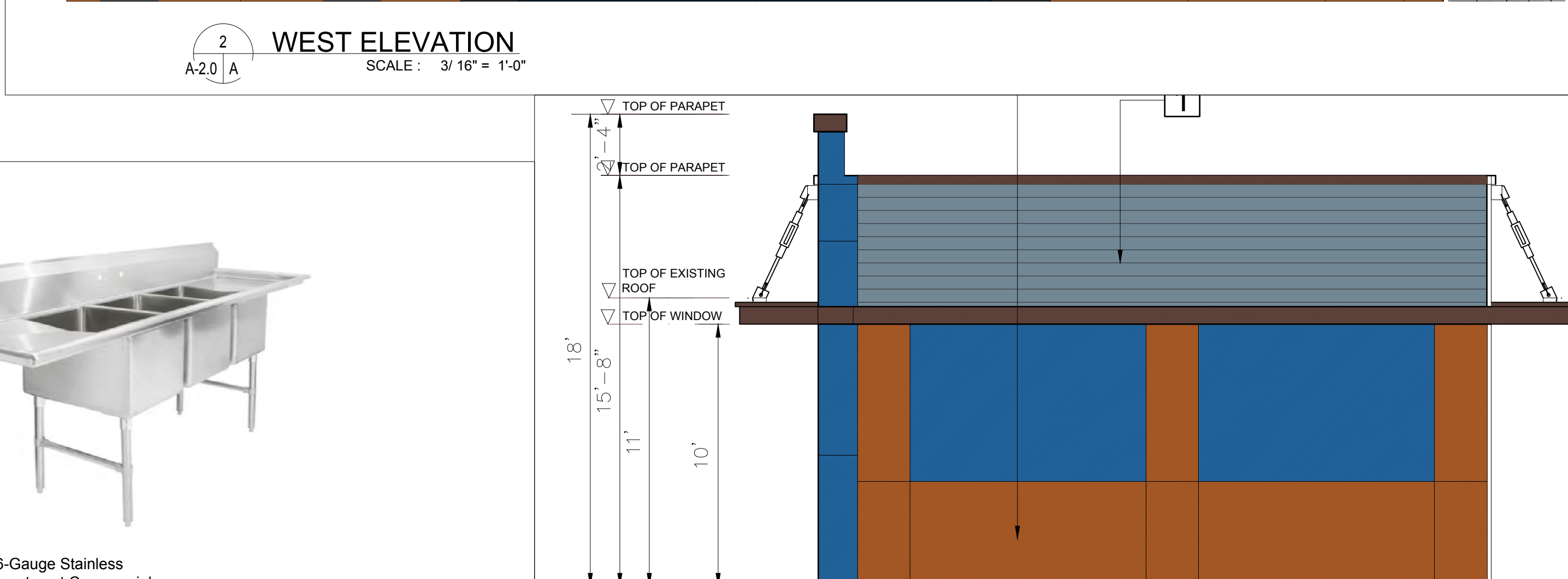
Manufacturer	Pitco
Baskets	2
Gas Type	NG
Number of Fry Pots	1
Type	Floor Model
Warranty	1-year Limited Parts & Labor
Width (in)	20
Oil Capacity/Fryer (lb)	90
Frypot Style	Tube Type
Food Type	Breaded
BTU per Fryer	140000
Series	Solstice™
Product	Fryers
Product Type	Cooking Equipment
Weight	226.00

**True Refrigeration T-49F-HC Specifications**

Manufacturer	True Refrigeration
Model	T-49HC
Width (in)	36
Depth (in)	24
Height (in)	83.38
Weight	60
Interior Construction	Aluminum
Sections	2
Supports	Casters
Thermometer Type	Digital
Type	Reach In
Voltage	115
Variants	5-year Parts, 7-year Compressor Parts
Width (in)	54.13
Style	T Series
Refrigerant	R290 (hydrocarbon)
Phase	1

**Advance Tabco CH-4848 Specifications**

Quantity	1/Each
Shipping Weight	160 lb.
Length	48 Inches
Depth	48 Inches
Height	20 Inches
Application	Non-Grease
Features	Made in America
Installation Type	Overhead
Material	Stainless Steel
Stainless Steel Type	Type 300
Style	Condensate Hoods
Type	Hoods



**Regency 106" 16-Gauge Stainless Steel Three Compartment Commercial Sink with 2 Drainboards - 18" x 24" x 14" Bowls**

**Compartment Sinks**  
Choosing the right sink for your needs

Regency supplies and equipment are designed strongly emphasizing quality and high-level performance.

- Relaxed, Rounded Edge**: Front and side bowl overhangs help prevent splatters and spills.
- 304 Steel Type**: Provides excellent durability and a smooth, professional finish.
- NSF Approved Interior**: Aluminum liner and stainless steel floor with covered corners make interior easy to keep clean.
- Easy to Clean**: Bottom-mounted condensing unit runs in the coolest part of the kitchen, a setup that preserves energy efficiency and reduces wear and tear on the equipment.

REGENCY Series	REGENCY Series	REGENCY Series
16 Gauge	16 Gauge	16 Gauge
Stainless Steel Type	304	304
Leg Construction	Customized Stainless Steel legs	Stainless Steel legs
Feet Construction	Stainless Steel Solid Feet	Stainless Steel Solid Feet
Over Bracing	No	No
Warranty	NSF/ANSI	NSF/ANSI

**LICENCED ARCHITECT**  
ROBERT A. MASSETTI  
C-12648  
06-30-2025  
Robert Massetti

**PROJECT DIRECTOR:**  
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**PROJECT:**  
734 SOUTH STATE STREET UKIAH,  
CA 95462

REVISIONS:	DATE:	DESCRIPTION:

**CURRENT ISSUE DATE:**  
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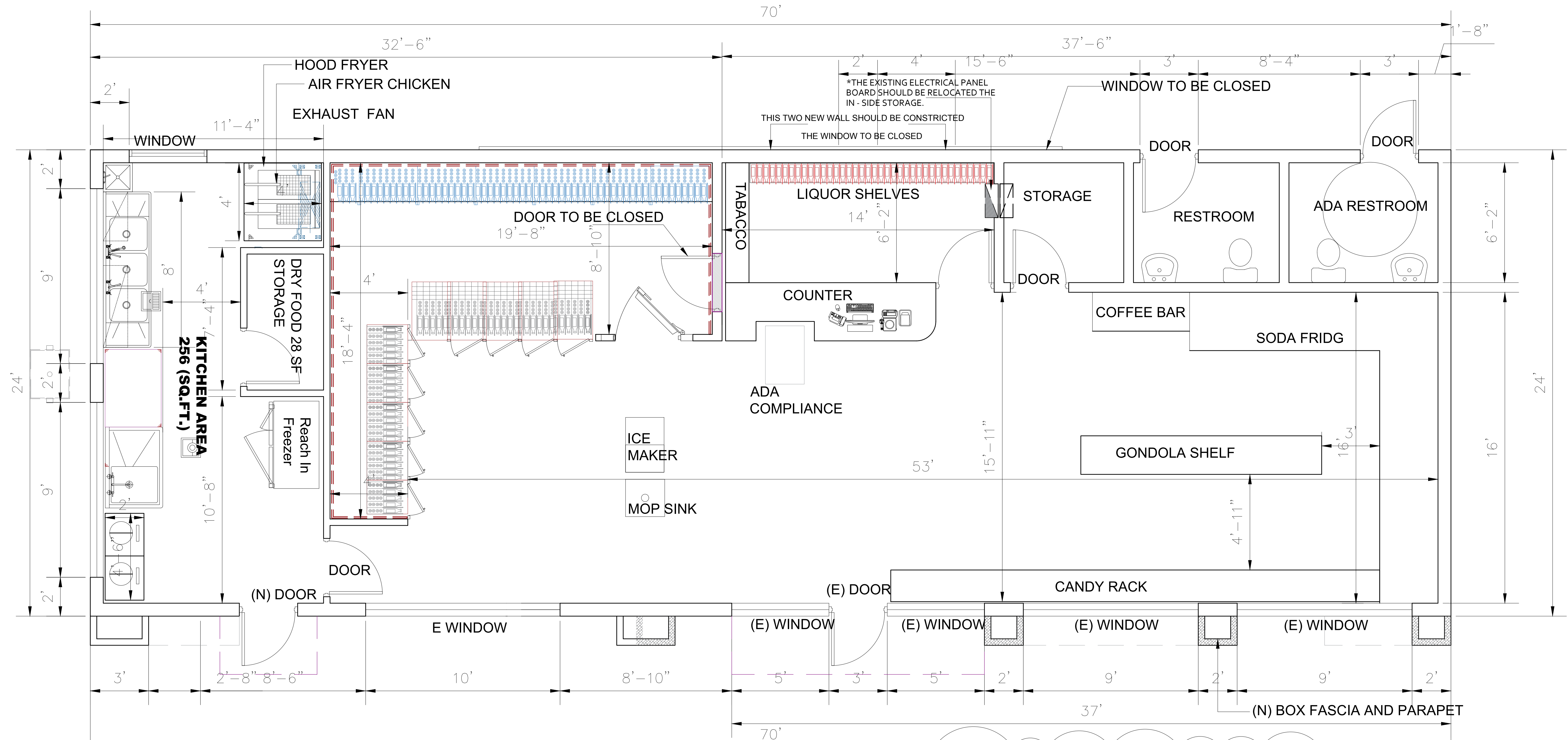
**ISSUED FOR:**  
01,24,2024

**CONTROL NO.:**

**DRAWN BY:** BOULEVARD  
**CHECKED:**  
**APPROVED:**

**SHEET TITLE:**  
PROPOSED PLAN & ELEVATIONS

**SHEET #**  
A-2.0

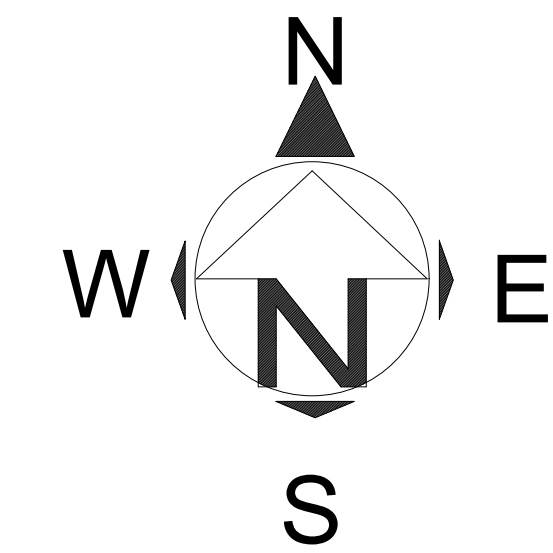


**ENLARGED FLOOR PLAN**

SCALE : 3/8" = 1'-0"

N STOR FRONT GLASS SYSTEM WITH 6' DOULE DOORS

**KITCHEN NOTES:**  
 KITCHEN IS ADDED TO THE EXISTING STORE WITH THE FOLLOWING KITCHEN ITEMS:  
 A. FRYER  
 B. HOOD  
 C. 3-COMP SINK  
 D. HAND WASH  
 E. FREEZER  
 F. DRY STORAGE  
 G. WORKING TABLE  
 kitchen operation hours:  
 6:00 AM-10:00 PM  
 KITCHEN TYPE:  
 CRUNCHY CHICKEN(FAST FOOD)



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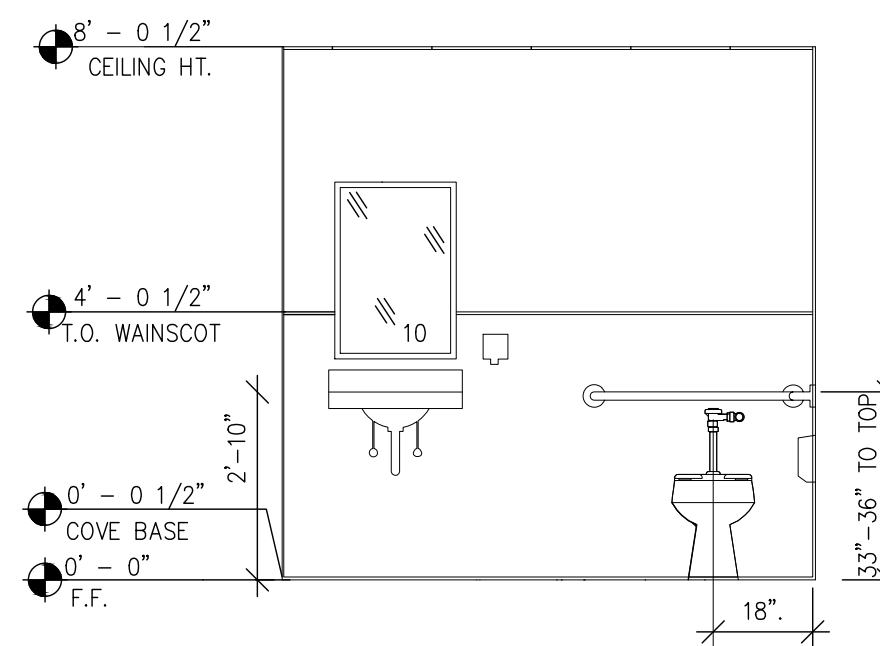
DRAWN BY: BOULEVARD  
 CHECKED:  
 APPROVED:

SHEET TITLE:  
 ENLARGED FLOOR PLAN

SHEET #  
 A-2.1

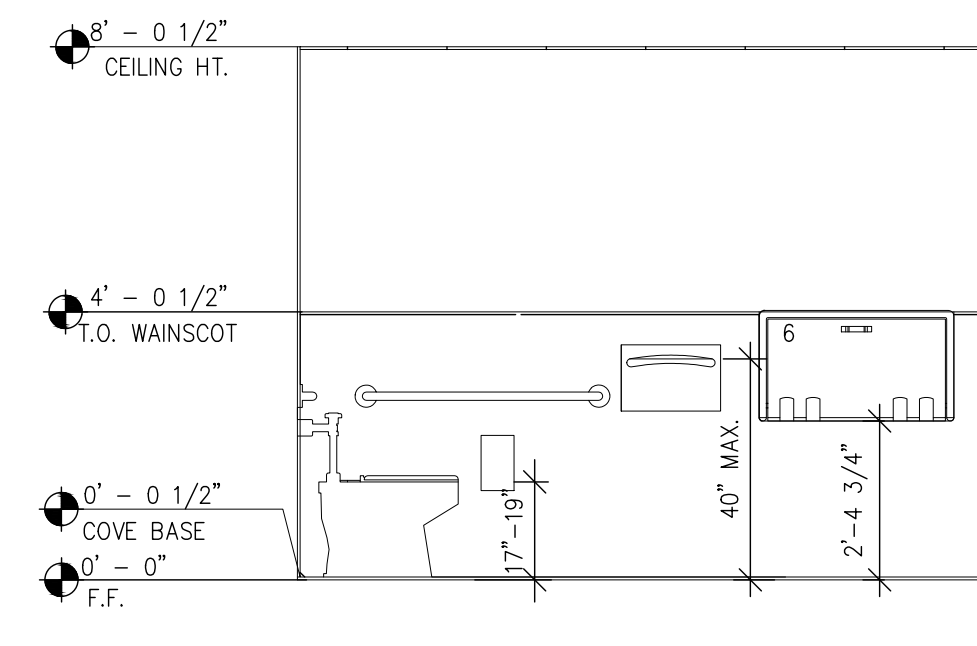
**GENERAL RESTROOM NOTE:**

CENTER TILES ON EACH WALL NO SMALL SLIVERS OF TILE IS ALLOWED. LAYOUT IS SHOWN. SMALLEST CUT TILE SHOULD NOT BE LESS THEN 4"



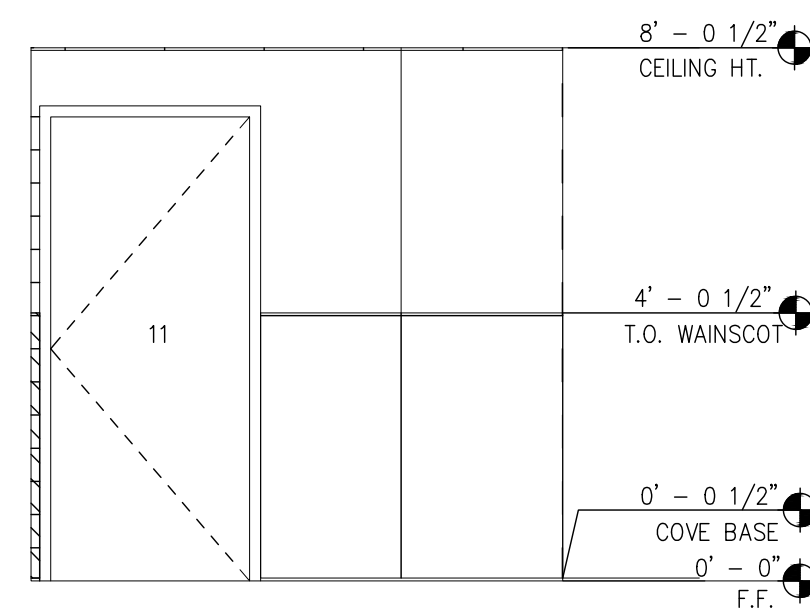
**10 MEN'S EAST ELEVATION**

3/8" = 1'-0"



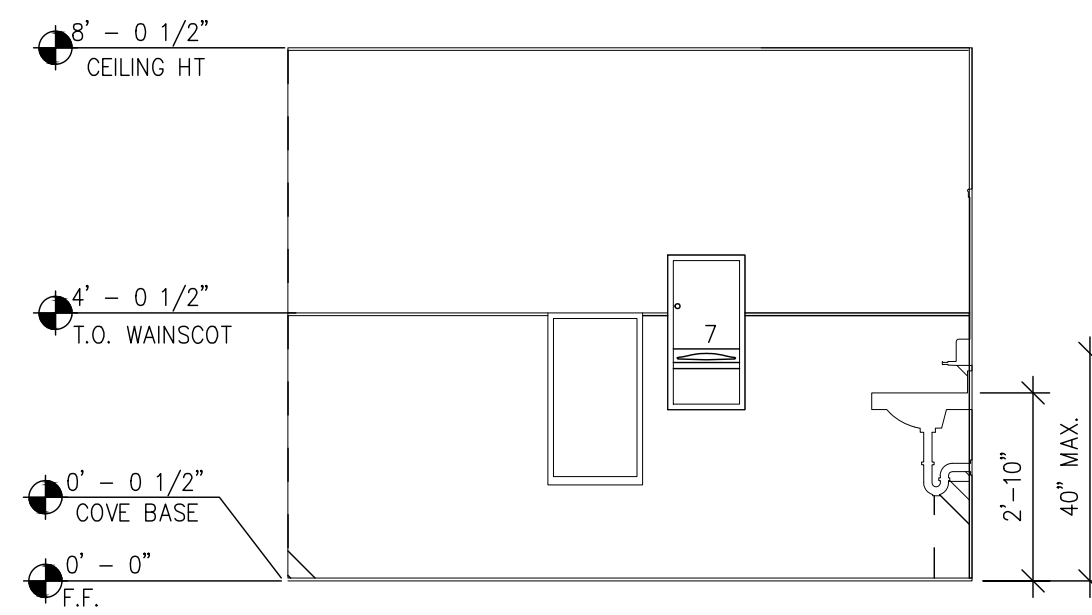
**6 MEN'S NORTH ELEVATION**

3/8" = 1'-0"



**11 MEN'S WEST ELEVATION**

3/8" = 1'-0"

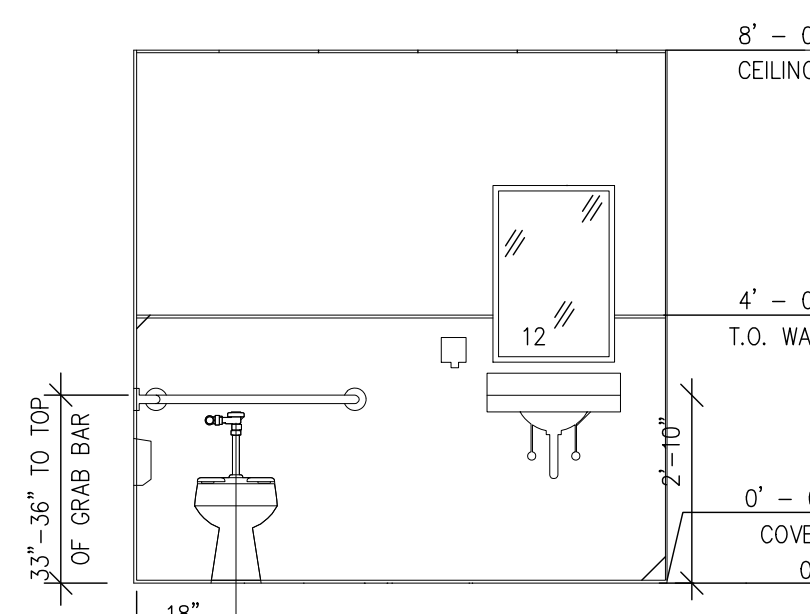


**8 MEN'S SOUTH ELEVATION**

3/8" = 1'-0"

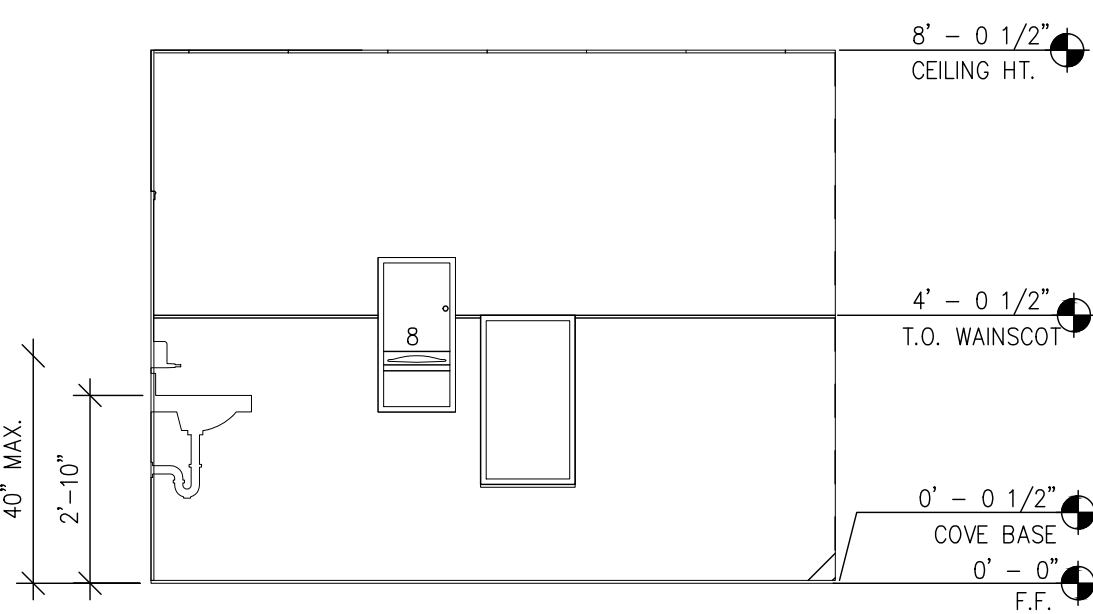
**GENERAL RESTROOM NOTE:**

CENTER TILES ON EACH WALL NO SMALL SLIVERS OF TILE IS ALLOWED. LAYOUT IS SHOWN. SMALLEST CUT TILE SHOULD NOT BE LESS THEN 3-1/2"



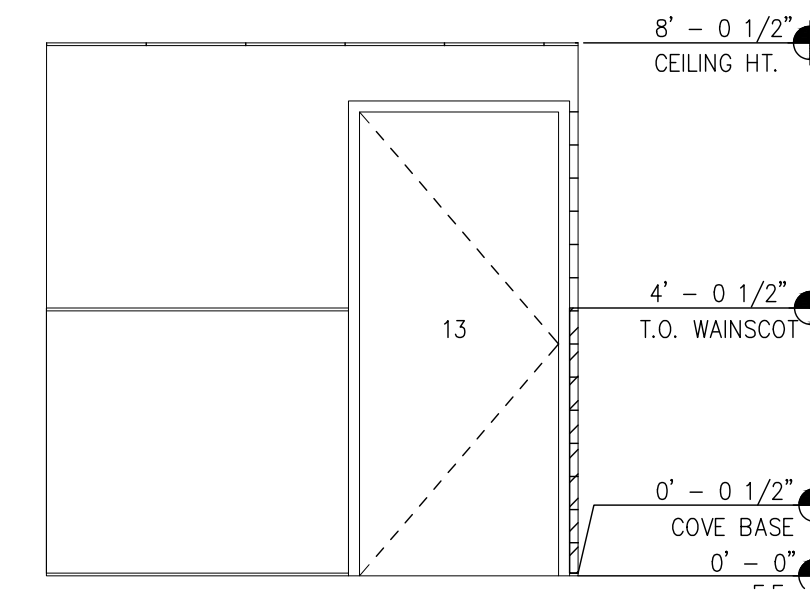
**12 WOMEN'S SOUTH ELEVATION**

3/8" = 1'-0"



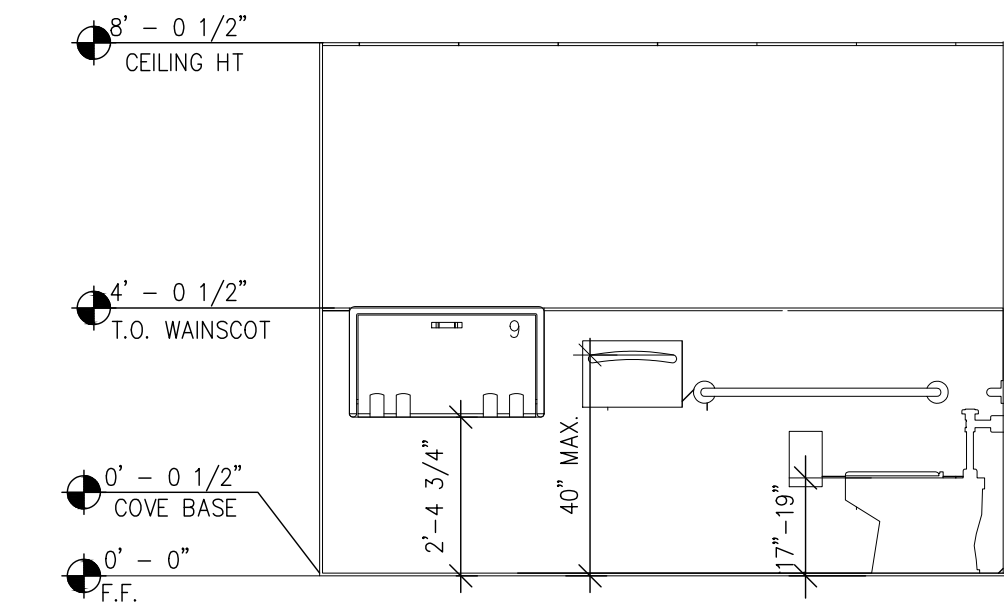
**8 WOMEN'S WEST ELEVATION**

3/8" = 1'-0"



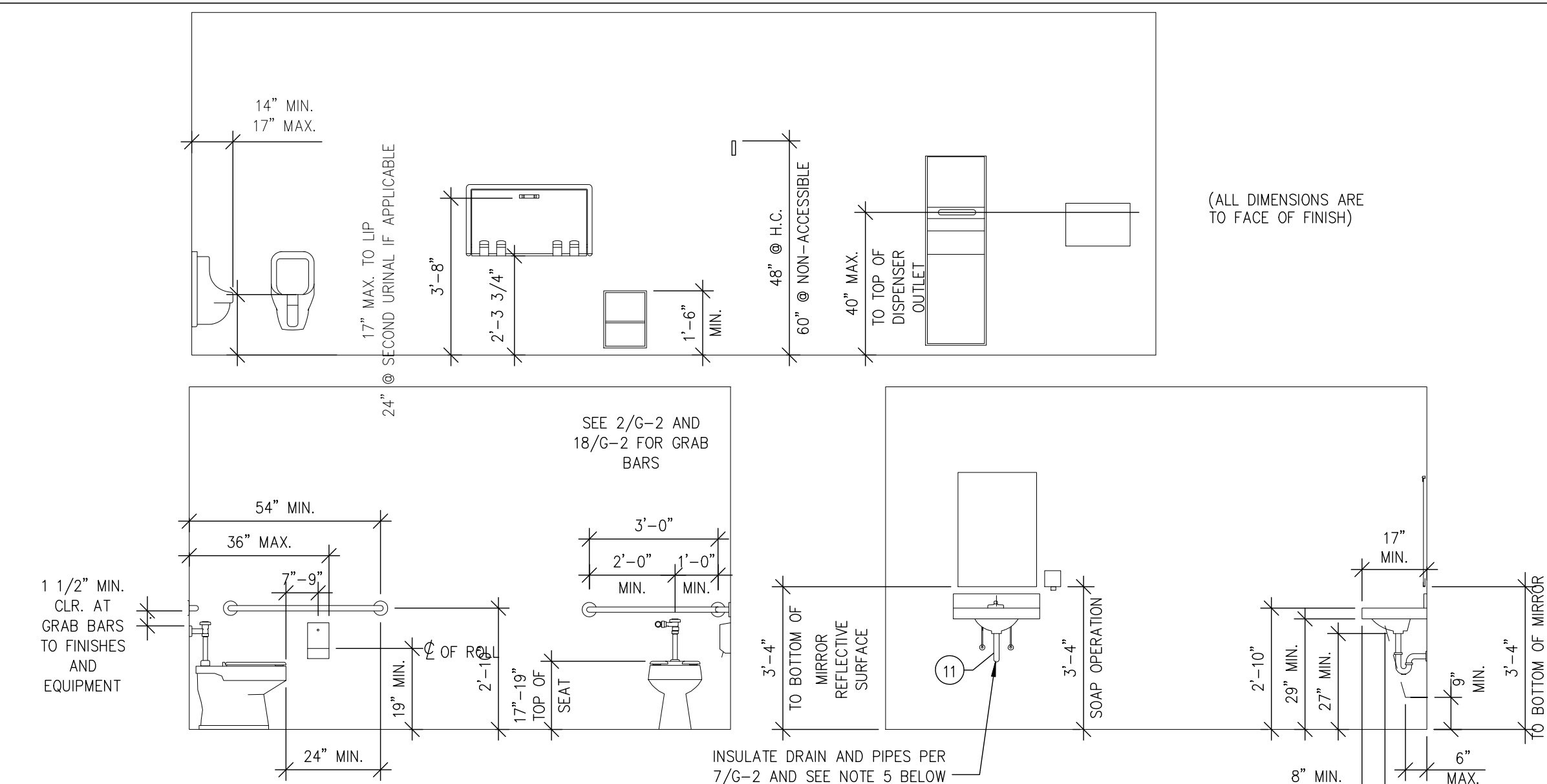
**13 WOMEN'S NORTH ELEVATION**

3/8" = 1'-0"



**9 WOMEN'S EAST ELEVATION**

3/8" = 1'-0"



**RESTROOM EQUIPMENT ELEVATION**

3/8" = 1'-0"

RESTROOM	MANUFACTURER	TILE	SIZE	LATICRETE SPECTRALOCK PRO PREMIUM GROUT	JOINT
WALL	CROSSVILLE "MAIN STREET"	CINEMA CHAMPAGNE - AV211	6" x 18"	#17 MARBLE BEIGE	3/16"
WAINSCOT	CROSSVILLE "MAIN STREET"	BISTRO BROWN - AV213	6" x 18"	#35 MOCHA	3/16"

- NOTES:
- OFFSET WALL TILES 50%
  - SEE A07 FOR CHEVRON SUPPLIER CONTACTS
  - SEE ELEVATIONS FOR THE LAYOUT. NO NARROW SLIVERS OF TILE WILL BE ACCEPTED.

**4 TILE SCHEDULE**

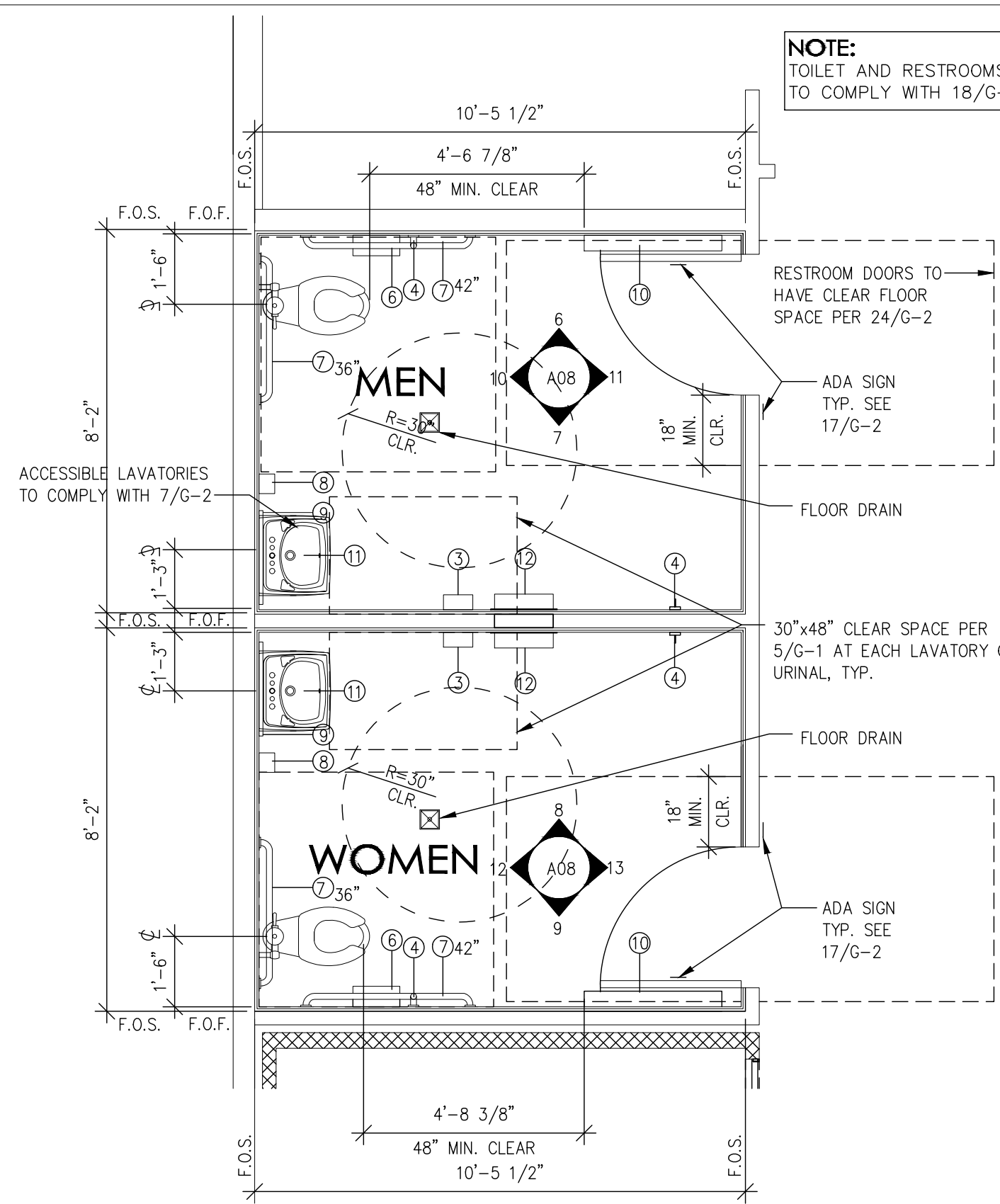
NO SCALE

**GENERAL NOTES**

- BATHROOM ACCESSORIES, SUCH AS GRAB BARS, PAPER TOWEL DISPENSERS, ETC., ON OR WITHIN WALLS SHALL BE SEALED AGAINST MOISTURE.
- INSTALL BATHROOM ACCESSORIES IN ITS PROPER LOCATION, FIRMLY ANCHORED INTO POSITION, LEVEL AND PLUMB, AND IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
- THE CHANGING TABLE, RESTROOM ACC. WOMEN AND MEN BY UNIVERSAL CAN BE OBTAINED AT CHEVRON NEGOTIATED PRICE. CONTACT: KEVIN PERLMUTTER, PHONE: (800) 764-3454 EXT. 251 SAP # 1134005, 1134006
- RESTROOM SIGNS, ADA SQUARE MEN/WOMEN SIGNS AND CA ROUND & TRIANGLE MEN/WOMEN SIGNS. SEE 17/G-2.
- LAUGUARD BY TRUEBRO CAN BE OBTAINED AT CHEVRON NEGOTIATED PRICE. CONTACT: (800) 340-5969
- PROVIDE BOTH FEDERAL ACCESSIBLE RESTROOM SIGN ON WALL AT SIDE OF DOOR AND CALIFORNIA GEOMETRIC SYMBOL SIGN ON DOOR, SEE 17/G-2

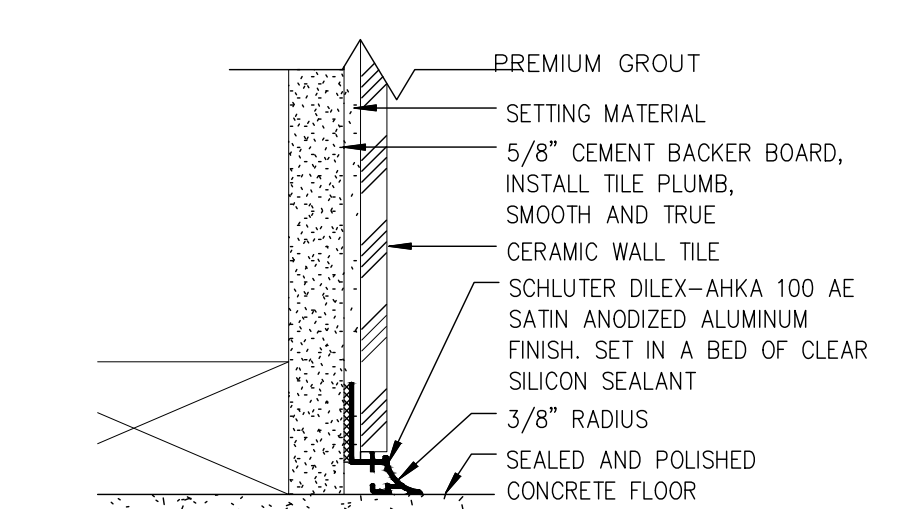
**RESTROOM EQUIPMENT LIST**

MARK	ITEM	DESCRIPTION	QTY
1	PAPER TOWEL/TRASH RECEPTACLE - SEMI RECESSED	BRADLEY #234	1
2	SURFACE MOUNTED SAN. NAPKIN DISPOSAL	BRADLEY # 4781-15	1
3	PAPER TOWEL DISPENSER	BRADLEY # 247	2
4	COAT HOOK	BRADLEY # 9114	2
5	TOILET SEAT COVER DISPENSER	BRADLEY # 5831	2
6	TOILET TISSUE DISPENSER	BRADLEY # 5402	2
7	GRAB BARS	BRADLEY # 812-001-36 & 812-001-42	2
8	SOAP DISPENSER	BRADLEY # 6536	2
9	MIRROR	SENTRY MIRROR SYSTEM WITH REPLACEABLE VANDAL PROOF PLEXI-SHIELD. SIZE 24" x 36" TO ORDER. PHONE: (877) 937-4435 FAX: (714) 835-3058	2
10	INFANT CHANGING TABLE	KOALA BABY KARE DIAPER CHANGER, MODEL # KB200-00	2
11	LAUGUARD SEE NOTE #5	TRUEBRO	2
12	WASTE RECEPTACLE	BRADLEY RECESSED TRASH RECEPTACLE MODEL #334	1
13	ALTERNATE AIR DRYER (NOT FOR COCO SITES)	WORLD DRYER "SLIM DRI" 120V 15 AMP CIRCUIT REQUIRED	1



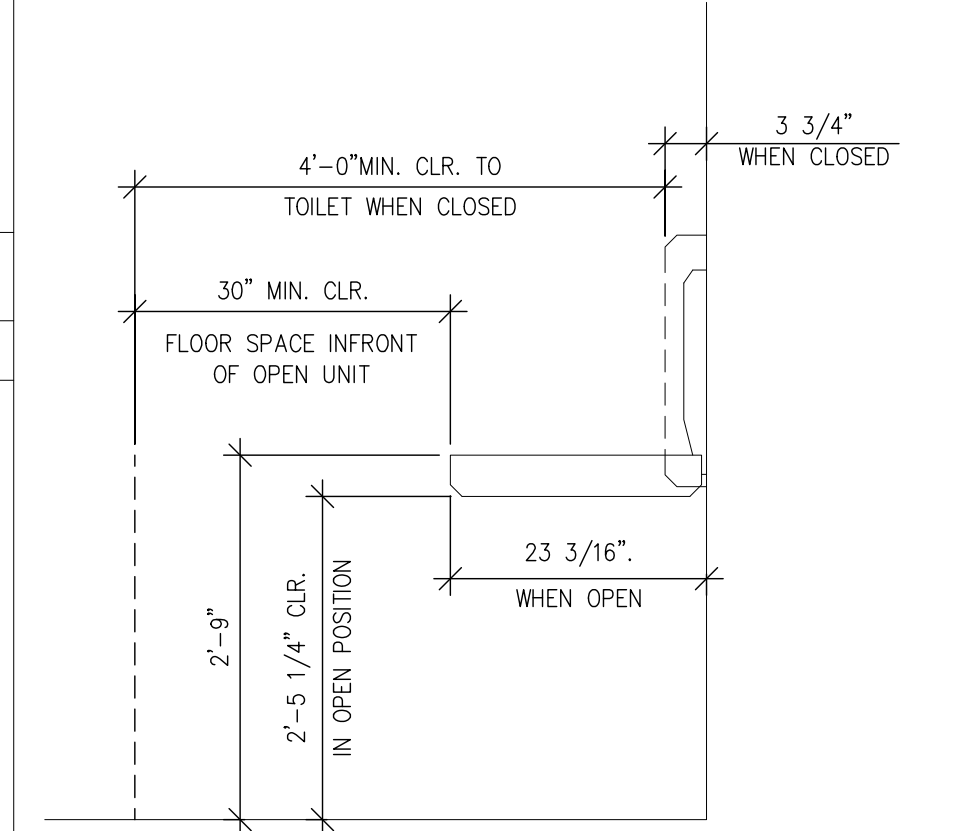
**5 RESTROOM PLAN**

3/8" = 1'-0"



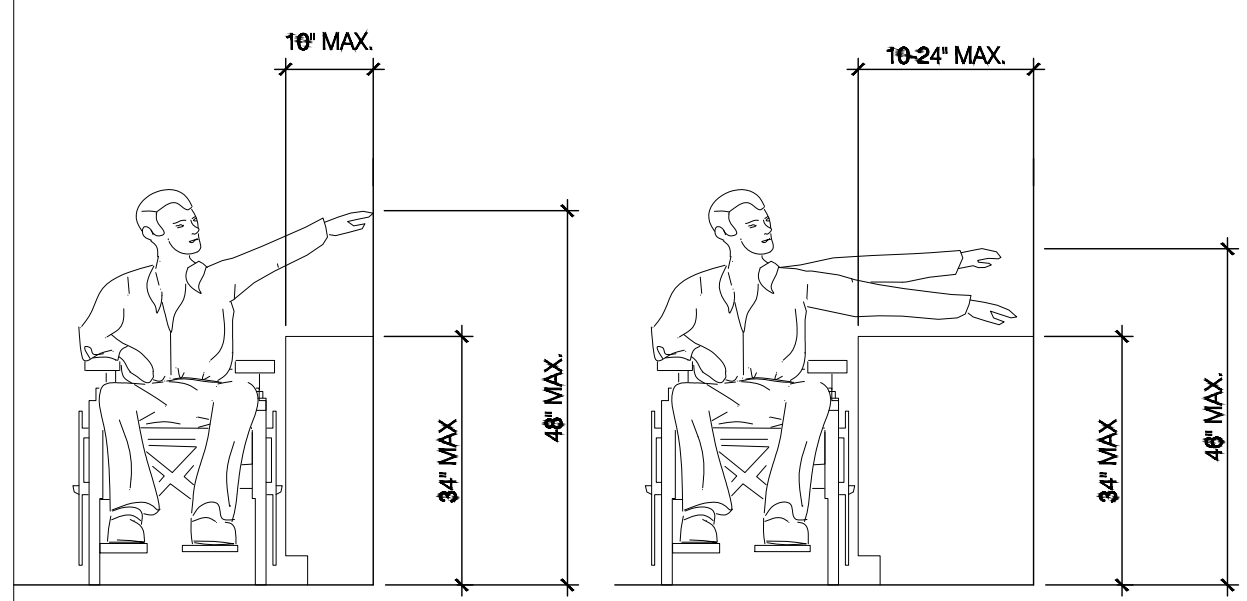
**1 SCHLUTER AHKA 100**

6" = 1'-0"



**2 BABY CHANGING TABLE**

3/4" = 1'-0"



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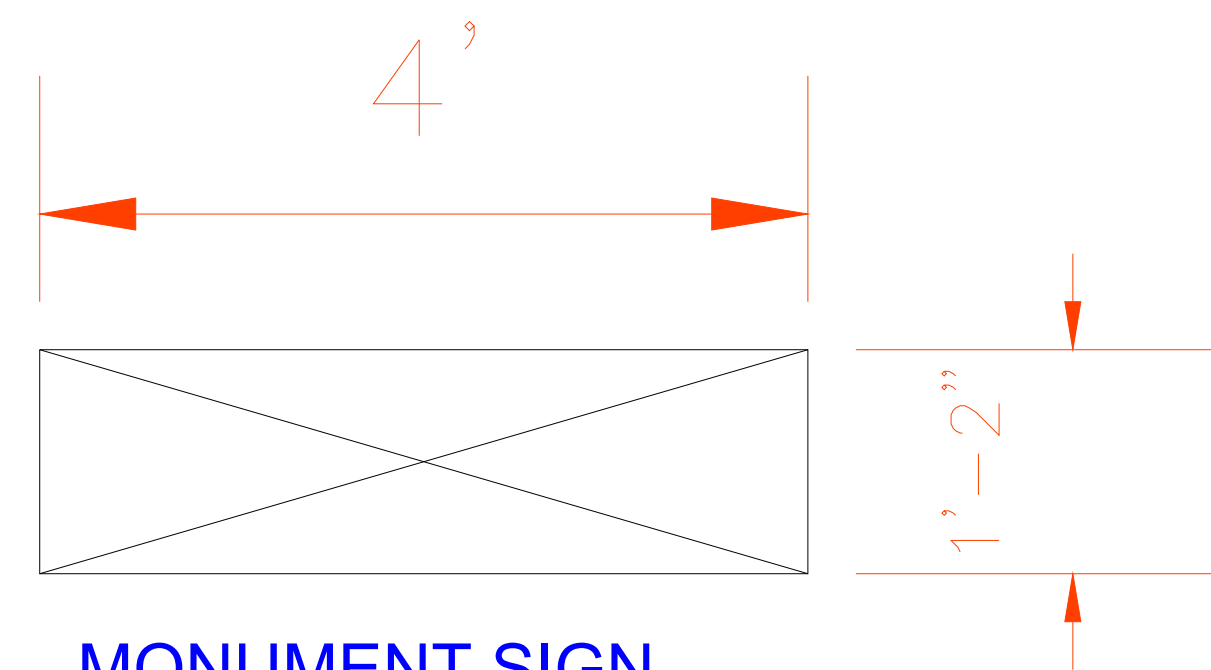
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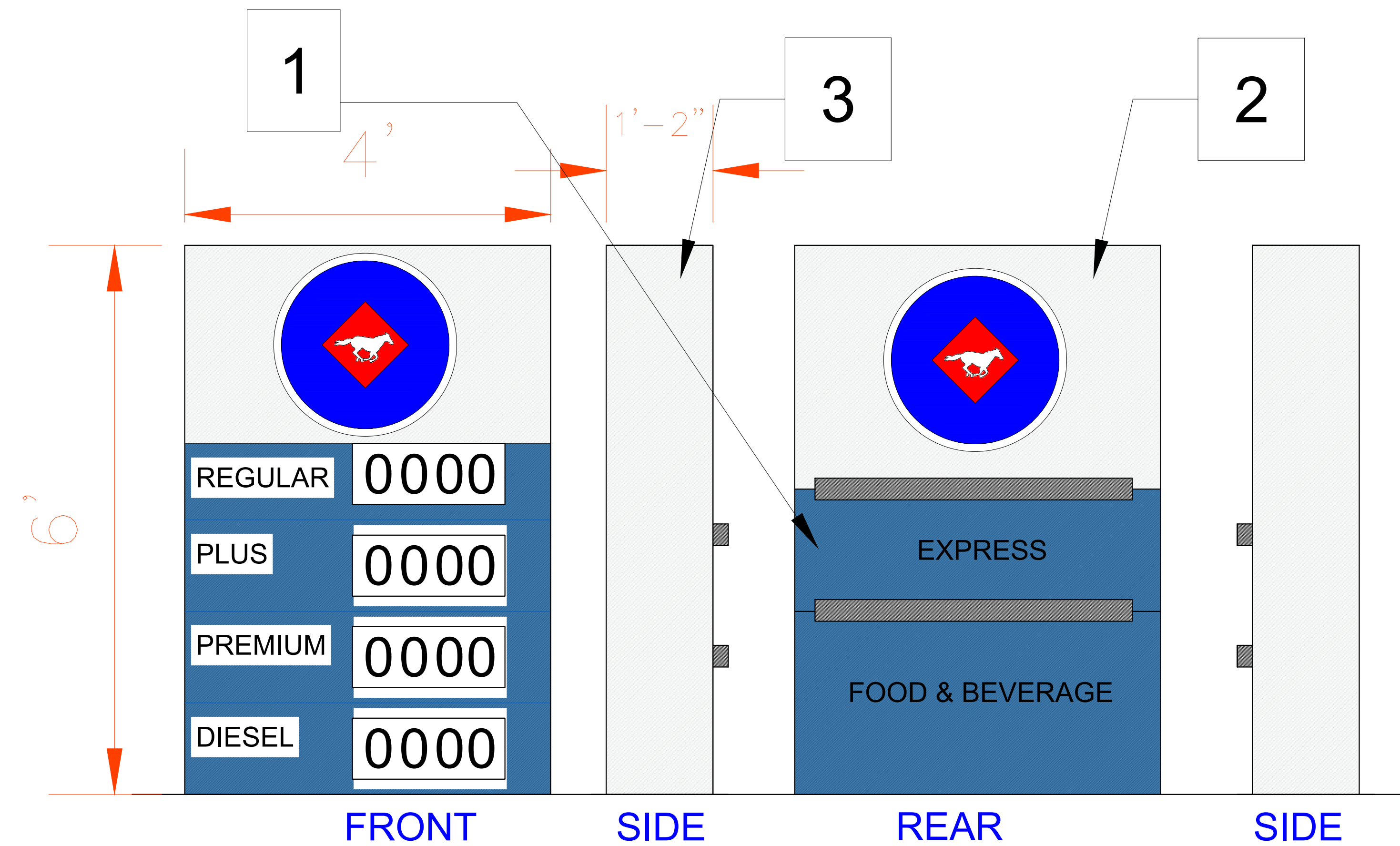
DRAWN BY: BOULEVARD  
 CHECKED:  
 APPROVED:

SHEET TITLE  
 TYPICAL RESTROOM DETAILS

SHEET #  
**A-3.0**

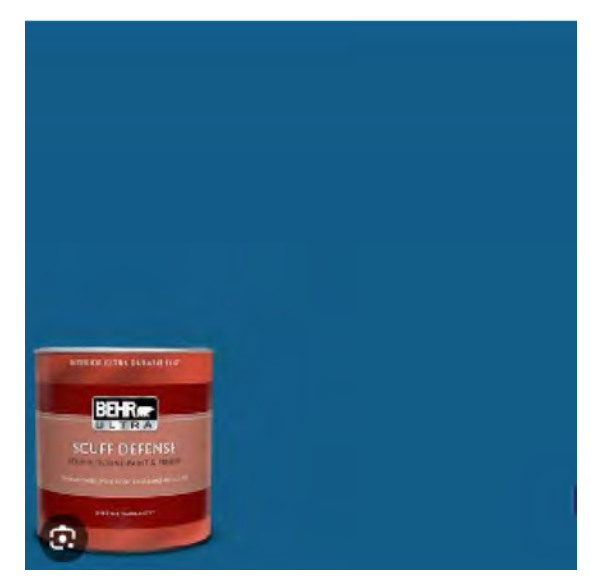
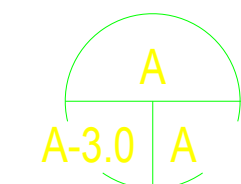


MONUMENT SIGN  
TOP VIEW:



FRONT  
MONUMENT SIGN

SCALE: 1" = 1'-0"



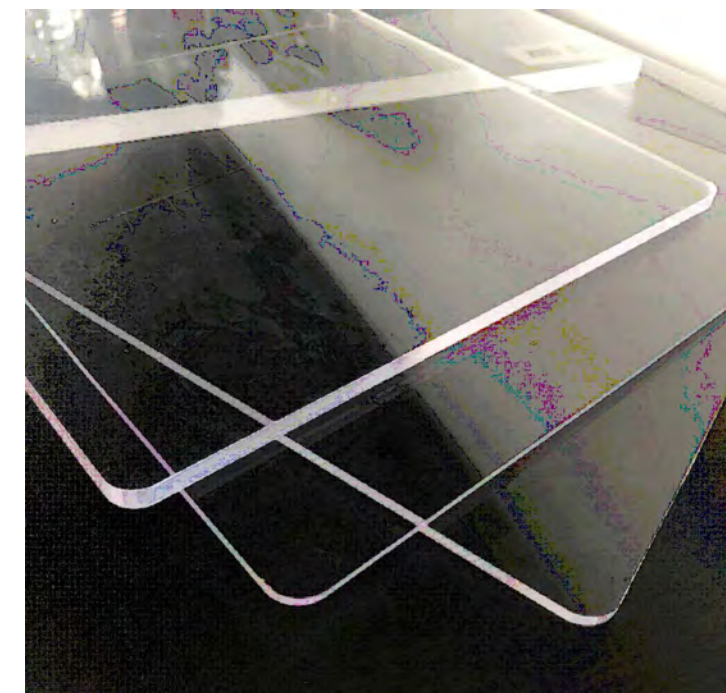
#PPU15-03 Dark Cobalt Blue  
Extra Durable Flat Interior Paint  
& Primer

1 FOR COLOR



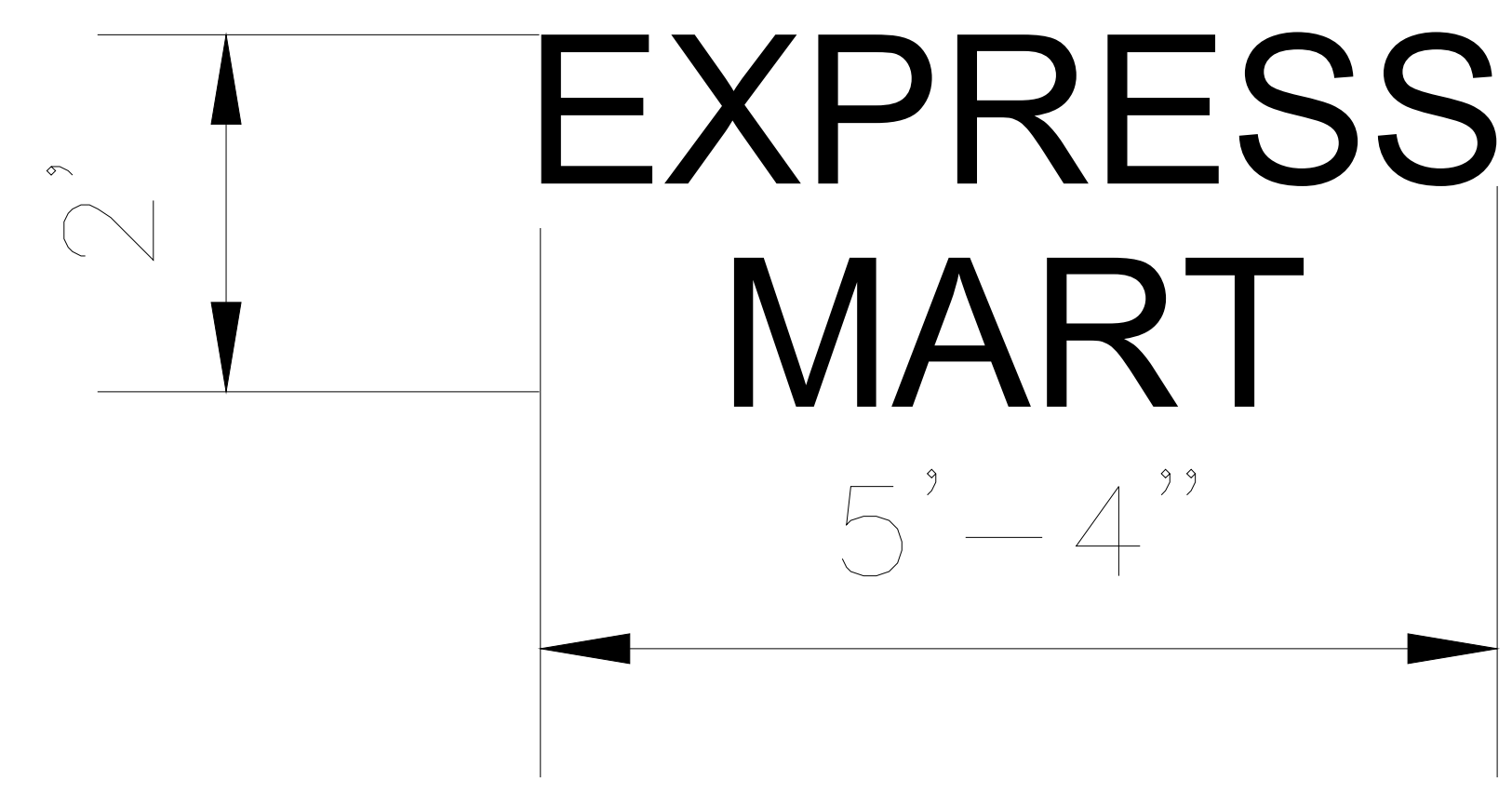
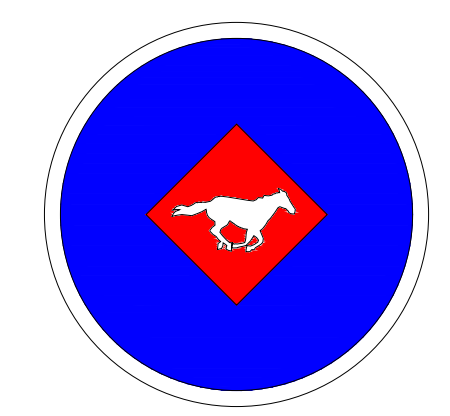
VIBRANT WHITE  
BWC-12

2 FOR COLOR



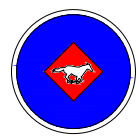

ACRYLIC SHEET

2 FOR MATERIAL



BUILDING SIGN:

1"=1'-0"

CANOPY SIGN :					
NO.	SIGN	FORNAME OF SIGN AND MARK	SIZE	NUMBERS	SQUARE FEET
A1	CANOPY SIGN	EXPRESS	6'-6"X1'	3	20 SF
			3.14 X (1) <sup>2</sup>	2	7 SF
BUILDING SIGN :					
A2	C-STORE SIGN	 EXPRESS MART	5'-4"X2'-0"	1	11
			3.14 X (1) <sup>2</sup>	1	3.14 SF
MONUMENT SIGN :					
A3	MONUMENT SIGN		6'X4'	2 SIDES	48 SF
TOTAL					90 SF



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PROJECT:  
734 SOUTH STATE STREET UKIAH,  
CA 95482

REVISIONS:	DATE:	DESCRIPTION:	BY:

CURRENT ISSUE DATE:  
01,24,2024

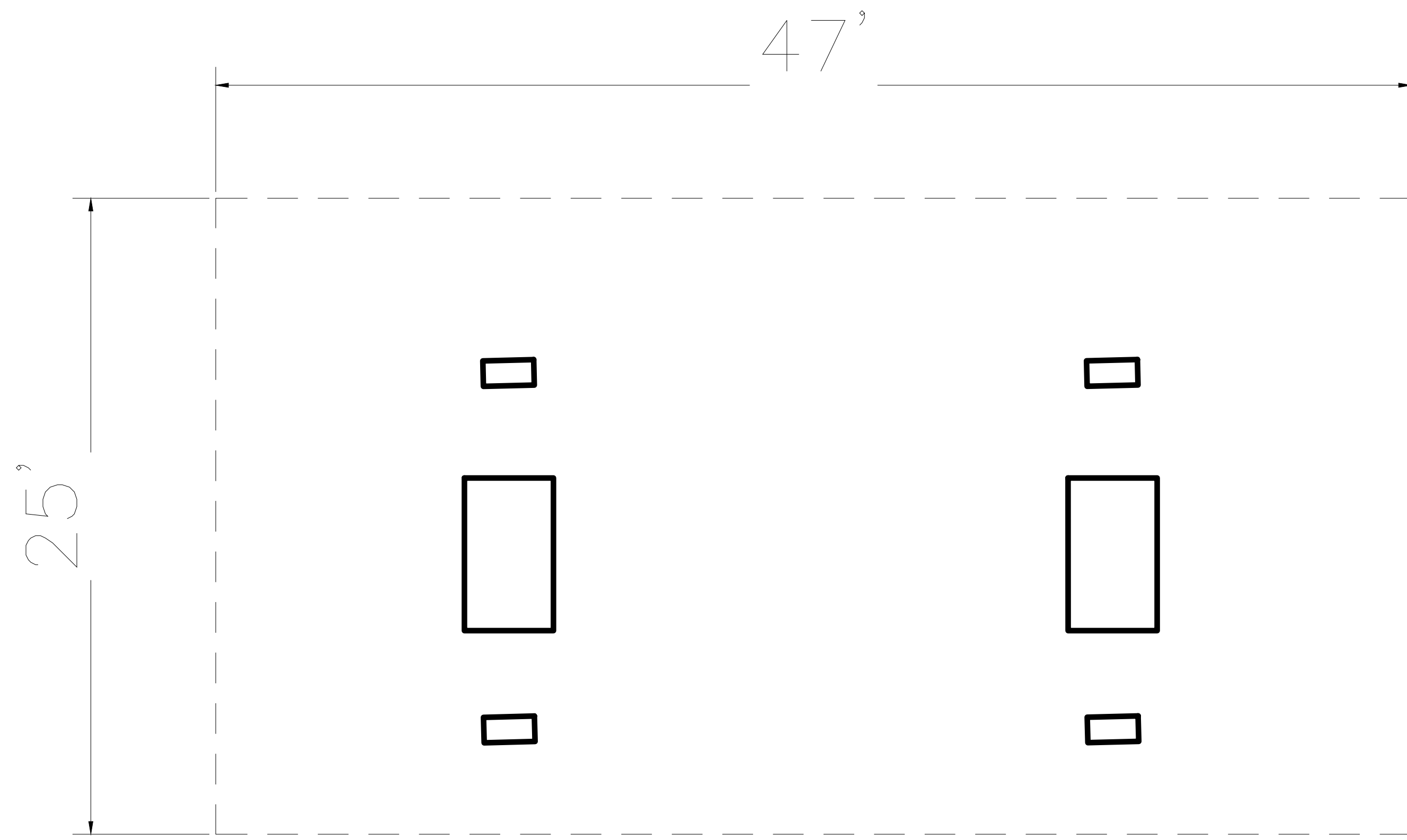
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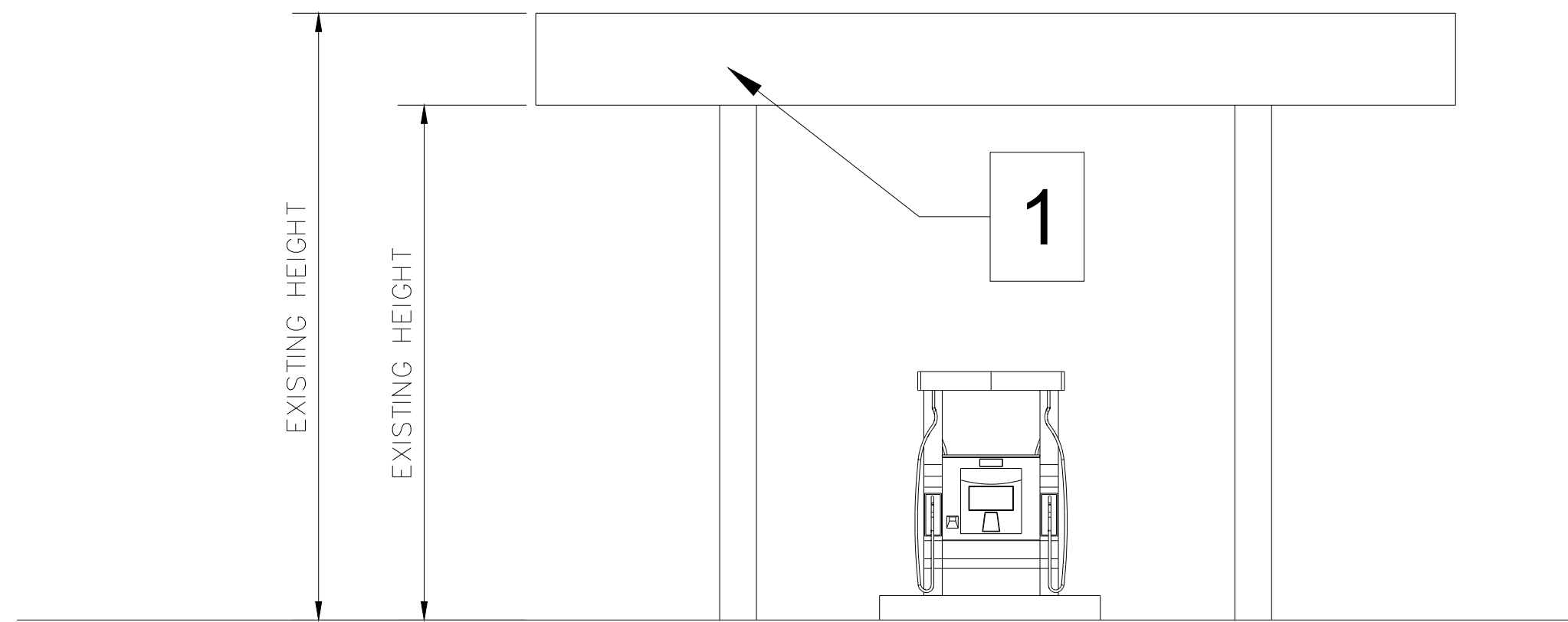
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APPROVED:      

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SIGNAGE DETAIL

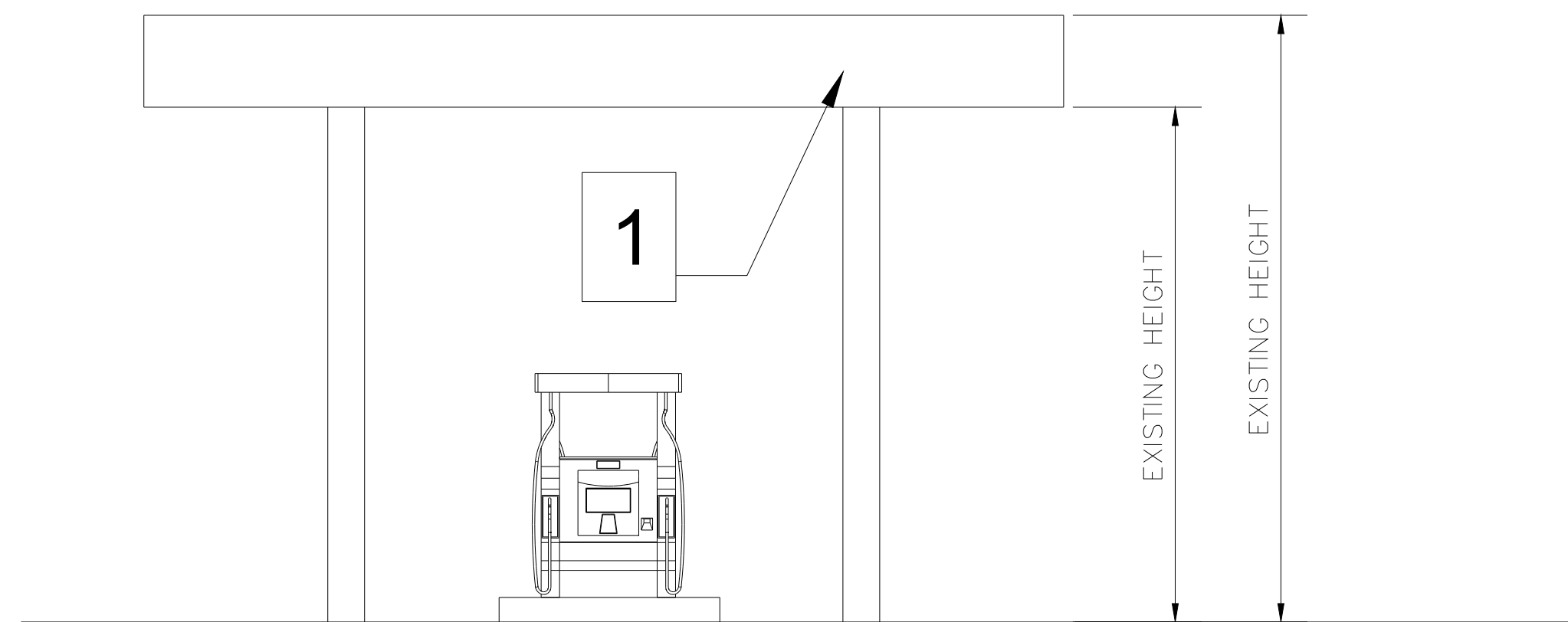
SHEET #  
A-4.0



**EXISTING GAS STATION FLOOR PLAN**  
SCALE: 1/4"=1'-0"



**EXISTING EAST ELEVATION**  
SCALE: 1/4"=1'-0"



**EXISTING WEST ELEVATION**  
SCALE: 1/4"=1'-0"



**EXISTING NORTH ELEVATION**  
SCALE: 1/4"=1'-0"



**EXISTING SOUTH ELEVATION**  
SCALE: 1/4"=1'-0"

# 1-EXISTING COLOR OF THE CANOPY



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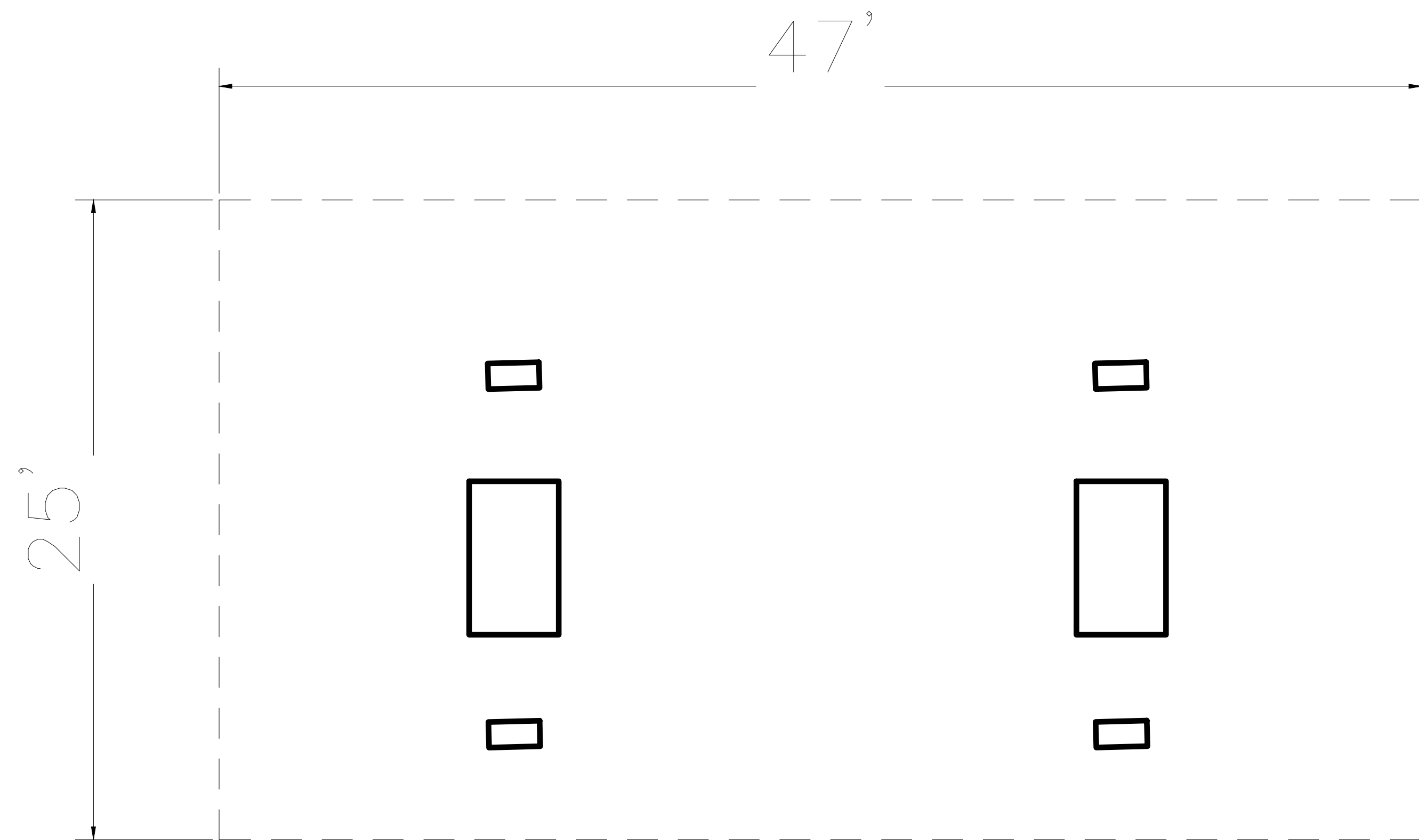
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**CHECKED:**

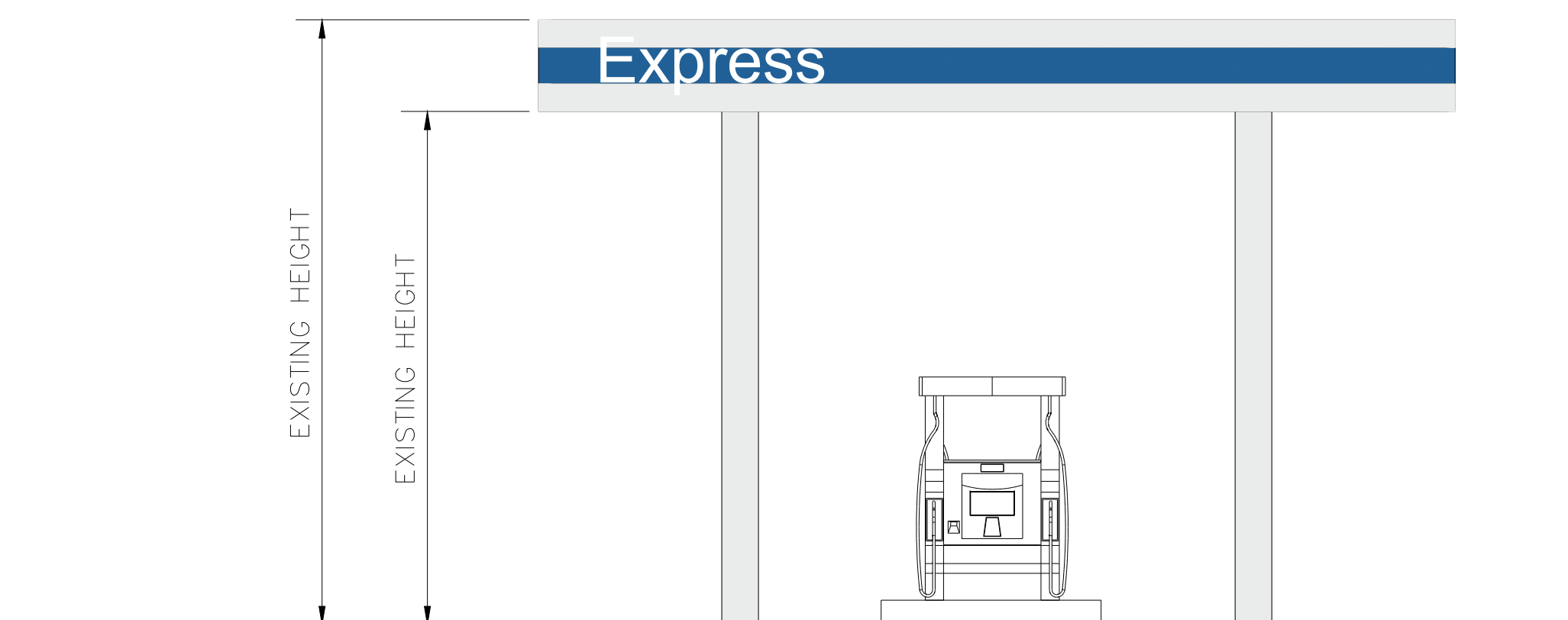
**APPROVED:**

**SHEET TITLE:**  
EXISTING GAS STATION PLAN AND ELEVATION

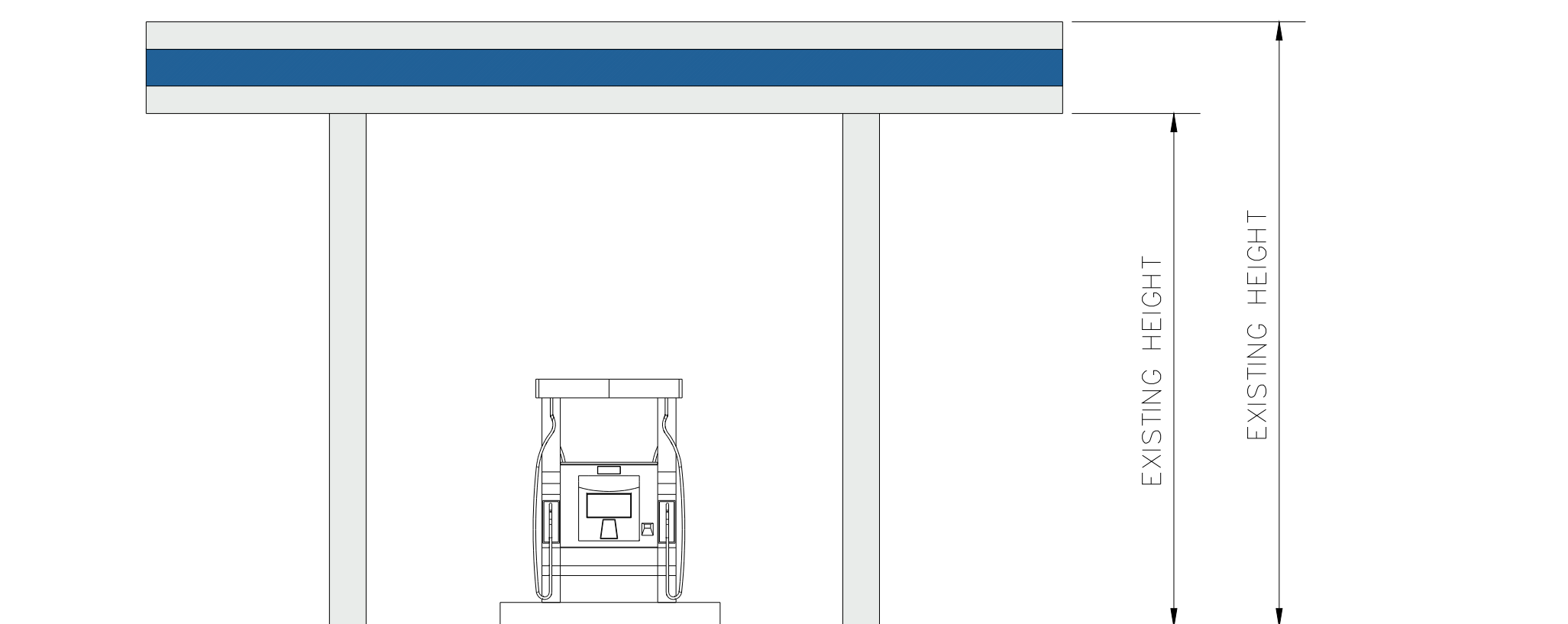
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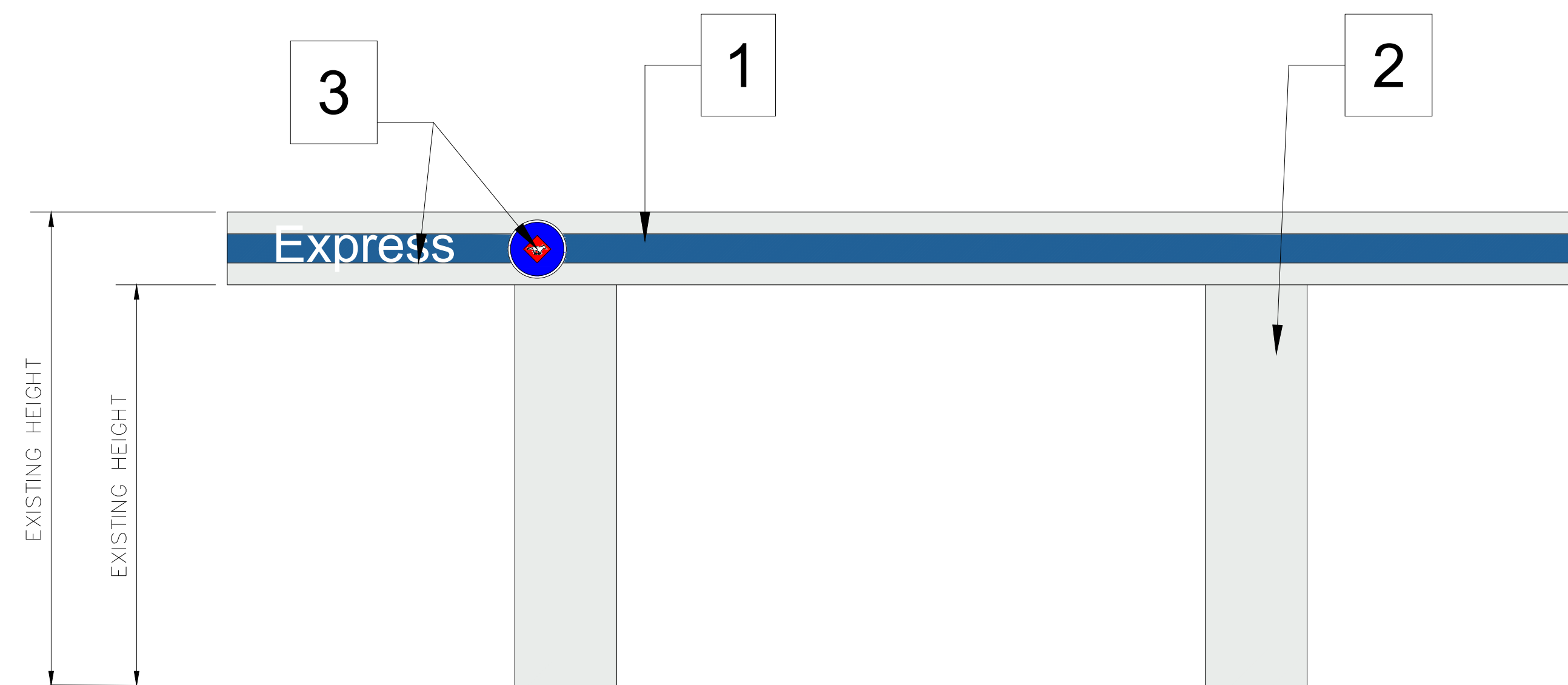
**GAS STATION FLOOR PLAN**  
SCALE: 1/4"=1'-0"



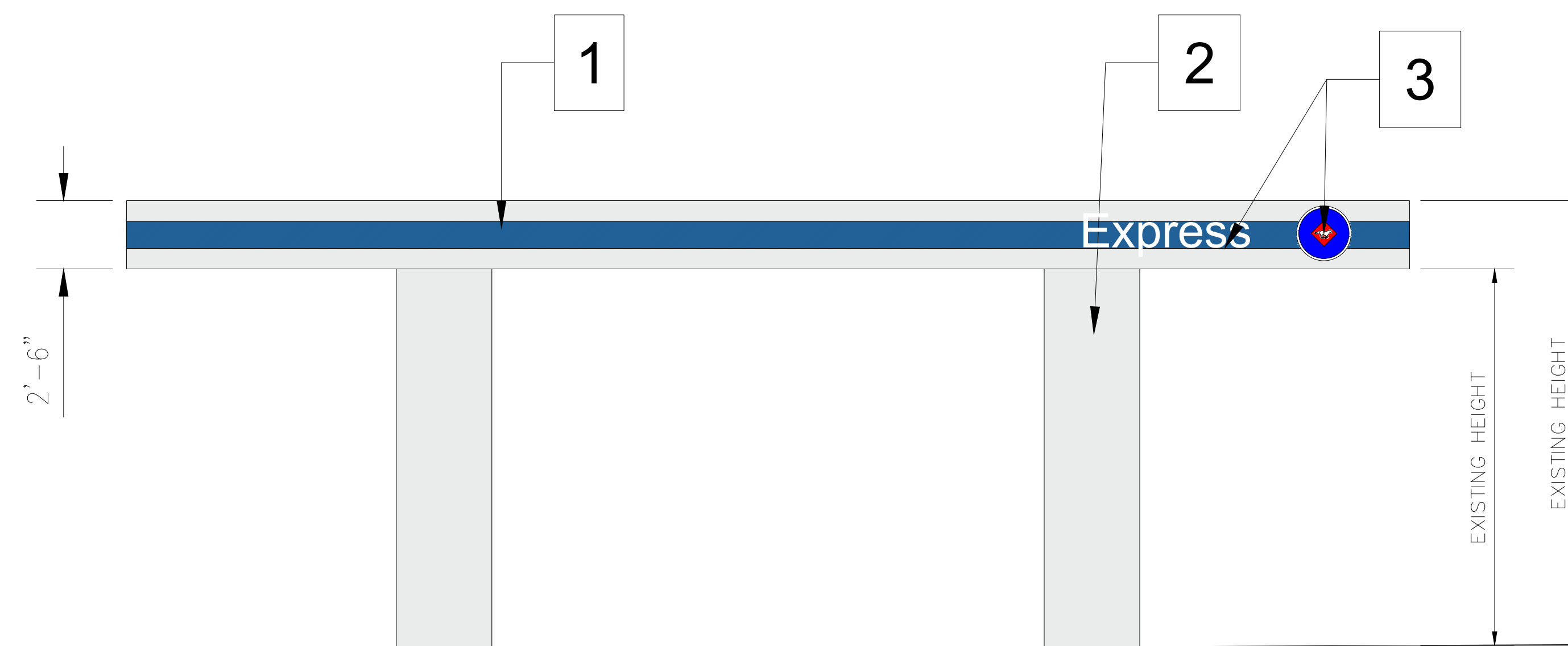
**EAST ELEVATION**  
SCALE: 1/4"=1'-0"



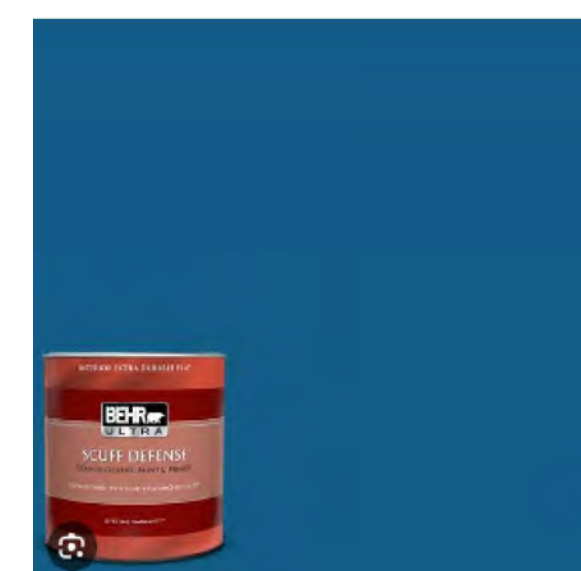
**WEST ELEVATION**  
SCALE: 1/4"=1'-0"



**NORTH ELEVATION**  
SCALE: 1/4"=1'-0"



**SOUTH ELEVATION**  
SCALE: 1/4"=1'-0"



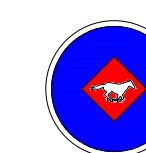
#PPU15-03 Dark Cobalt Blue  
Extra Durable Flat Interior Paint  
& Primer

**1** FOR COLOR



VIBRANT WHITE  
BWC-12

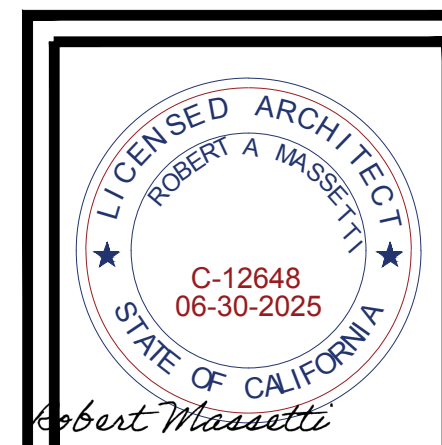
**2** FOR COLOR



**Express**

CANOPY LOGO AND SIGN

**3** SIGN



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APPROVED:

SHEET TITLE:  
PROPOSED GAS STATION PLAN  
AND ELEVATION

SHEET #  
**A-6.0**

## GENERAL NOTES & SPECS.

- PROVIDE ALL MATERIALS AND LABOR AS REQUIRED TO ACHIEVE A COMPLETE AND OPERATING SYSTEM.
- COORDINATE AND OBTAIN APPROVALS FROM ALL RESPECTIVE UTILITY COMPANIES AS REQUIRED FOR A COMPLETE AND OPERATING INSTALLATION.
- INSTALL RACEWAY SYSTEMS AS FOLLOWS:
  - USE RIGID GALVANIZED STEEL IN ALL AREAS EXPOSED TO WEATHER OR PHYSICAL DAMAGE.
  - USE FLEXIBLE METALLIC CONDUIT ONLY IN AREAS AS PERMITTED BY LOCAL CODE AUTHORITY. USE SEAL-TITE IN AREAS EXPOSED TO WEATHER.
  - USE COMPRESSION TYPE FITTINGS FOR ELECTRICAL METALLIC TUBING WHERE UTILIZED.
  - USE P.V.C. CONDUIT UNDERGROUND WITH CODE SIZED GROUND, CONDUIT RISERS ANS STUBS ABOVE GRADE SHALL BE I.M.C. WITH HALF-LAPPED TAPE COVERING OR P.V.C. COATING.
- ALL NEW WIRING SHALL BE COPPER TYPE "THHN/THWN" - U.O.N..
- ALL ROOF PENETRATIONS FOR MECHANICAL EQUIPMENT SHALL BE INSIDE CURBS. ALL CONDUITS SHALL BE ROUTED BELOW ROOF. REFERENCE MECHANICAL DRAWINGS FOR EXACT LOCATIONS.
- ALL FIXTURE, DEVICE, ETC... LOCATIONS SHALL BE VERIFIED WITH ARCH. DRAWINGS AS WELL AS EQUIPMENT SUPPLIER REQUIREMENTS PRIOR TO ANY ROUGH-IN WORK.
- ALL LIGHTING FIXTURES SHALL BE MOUNTED AND SUPPORTED IN ACCORDANCE WITH OSHA STANDARDS AND ALL NATIONAL AND LOCAL ELECTRICAL CODES.
- ELECTRICAL CONTRACTOR SHALL PROVIDE LIGHTING FIXTURE MOUNTING KITS AS REQUIRED TO SUIT THE EXACT TYPE OF CEILING TO WHICH THEY ARE MOUNTED.
- THESE DRAWINGS ARE DIAGRAMMATIC AND REPRESENT THE INTENT OF EQUIPMENT, DEVICES, ETC... TO BE CONNECTED AND THE CIRCUITS TO WHICH THEY ARE TO BE CONNECTED TO. CONTRACTOR SHALL INSTALL ALL CONDUIT, J-BOXES, ETC... AS REQUIRED FOR A COMPLETE AND OPERATING SYSTEM.
- ALL EXTERIOR EQUIPMENT SHALL BE WEATHERPROOF.
- ELECTRICAL CONTRACTOR SHALL PERFORM ALL WORK IN STRICT ACCORDANCE WITH ALL LOCAL AND NATIONAL GOVERNING CODES.
- ALL EQUIPMENT SHALL BE NEW AND BEAR A "UL" LABEL - U.O.N..
- ELECTRICAL CONTRACTOR SHALL SECURE AND PAY FOR ALL NECESSARY PERMITS.
- COMPLETE ELECTRICAL INSTALLATION SHALL BE GUARANTEED IN WRITING FOR A PERIOD OF (1) YEAR - U.O.N..
- ELECTRICAL CONTRACTOR SHALL INCLUDE IN BID - COSTS FOR ALL HVAC CONTROL COMPONENTS, CONDUITS, DEVICES, ETC... AS DEEMED NECESSARY FOR A COMPLETE AND OPERATING HVAC SYSTEM. REFER TO MECHANICAL DRAWINGS, DIAGRAMS AND SPECS FOR THOSE ITEMS REQUIRED UNDER THE ELECTRICAL SECTION OF THIS CONTRACT. CONTRACTOR TO PROVIDE MATERIALS AND CONNECTION TO FIRE SYSTEM AS SHOWN ON MECHANICAL DRAWINGS.
- ELECTRICAL CONTRACTOR SHALL VISIT SITE PRIOR TO BID DATE, TO VERIFY ALL EXISTING CONDITIONS TO BE ENCOUNTERED IN THE INSTALLATION OF ALL NEW EQUIPMENT, FIXTURES DEVICES, FEEDERS, ETC... EXACT INSTALLATION METHOD AND REQUIREMENTS SHALL BE VERIFIED AND DETERMINED PRIOR TO BID DATE. CONTRACTORS SHALL IMMEDIATELY NOTIFY THIS ENGINEER OF ANY REQUIRED MODIFICATIONS WHICH ARE NOT SHOWN ON THESE DRAWINGS. SUBMITTAL OF BID INDICATES CONTRACTOR IS COGNIZANT OF ALL JOB SITE CONDITIONS AND WORK TO BE PERFORMED.
- ALL EQUIPMENT ELECTRICAL CHARACTERISTICS, LOCATIONS, AND CONNECTION REQUIREMENTS SHALL BE VERIFIED PRIOR TO ANY ROUGH-IN WORK.
- ELECTRICAL CONTRACTOR SHALL FURNISH THE FOLLOWING SHOP DRAWINGS FOR PRIOR APPROVAL:
  - ALL SUBSTITUTED LIGHT FIXTURES.
  - ALL ELECTRICAL SERVICE EQUIPMENT, DISTRIBUTION EQUIPMENT AND PANELBOARDS.
  - FIRE ALARM SYSTEM. (SEE NOTE #23 BELOW).
  - OTHER ITEMS AS SPECIFICALLY INDICATED.

THESE ITEMS SHALL BE APPROVED BY THIS OFFICE PRIOR TO ANY COMMENCEMENT OF PLACING ORDERS OR PERFORMING ANY ROUGH-IN WORK.
- COMPLETE ELECTRICAL SYSTEM SHALL BE GROUNDED IN ACCORDANCE WITH THE PRESENTLY ADOPTED EDITION OF THE N.E.C. ART. 250.
- PENETRATIONS OF ALL FIRE RATED WALLS OR CEILINGS SHALL BE FIRE RATED IN ACCORDANCE WITH ALL LOCAL AND NATIONAL CODES.
- PROVIDE ENGRAVED PLASTIC NAMEPLATES FOR ALL MAJOR ELECTRICAL EQUIPMENT.
- PROVIDE THE OWNER AND THIS ENGINEER WITH ONE SET OF ELECTRICAL "AS-BUILTS" AT THE COMPLETION OF JOB.
- ELECTRICAL CONTRACTOR SHALL INCLUDE IN BID ALL COSTS FOR A COMPLETE AND OPERATING FIRE ALARM SYSTEM WHICH NOT ONLY SATISFIES THE FUNCTIONS AS DESCRIBED IN THE ELECTRICAL SPECIFICATIONS BUT ALSO SATISFIES ANY AND ALL LOCAL FIRE DEPARTMENT REQUIREMENTS. ELECTRICAL CONTRACTOR TO VERIFY ALL LOCAL FIRE DEPARTMENT REQUIREMENTS PRIOR TO BID AND INCLUDE ALL COSTS IN BASE BID. ELECTRICAL CONTRACTOR IS TO OBTAIN LOCAL FIRE DEPARTMENT SHOP DRAWING APPROVAL (IN WRITING) PRIOR TO SUBMISSION OF SHOP DRAWINGS TO ENGINEER FOR REVIEW.
- ALL VOLUME CONTROLS, THERMOSTATS, SWITCHES OR ANY DEVICES THAT ARE MOUNTED ABOVE THE CHAIR RAIL IN PUBLIC AREAS MUST BE MOUNTED AS CLOSE AS POSSIBLE TO DOOR OR WINDOW OPENINGS SO AS NOT TO INTERFERE WITH OWNER HUNG DECOR.
- ALL GASOLINE DISPENSIGN AND SERVICE STATIONS SHALL COMPLY WITH THE CURRENT NEC SECTION AND ARTICLE.

## ABBREVIATIONS

A.F.F.	ABOVE FINISH FLOOR	A	AMPS
U.O.N.	UNLESS OTHERWISE NOTED	GFI	GROUND FAULT INTERRUPTER
C.O.	CONDUIT ONLY W/PULL ROPE	GND.	GROUND
W.P.	WEATHER PROOF	V.L.	VERIFY LOCATION
CU.	COPPER	A.C.	ABOVE COUNTER
M.L.O.	MAIN LUGS ONLY	N.L.	NIGHT LIGHT
A.F.G.	ABOVE FINISH GRADE	EM.	EMERGENCY

## ELECTRICAL SYMBOLS LIST

	SURFACE WALL MTD. FLUORESCENT LIGHTING FIXTURE.
	SURFACE MOUNTED FLUORESCENT MODULAR TYPE LIGHTING FIXTURE. EM AND/OR SHADED CONNECTION POINT INDICATES FIXTURE WITH EMERGENCY BATTERY PACK. NL INDICATES FIXTURE ON NIGHT LIGHT CIRCUIT.
	RECESSED FLUORESCENT LIGHTING FIXTURE. EM AND/OR SHADED CONNECTION POINTS INDICATES FIXTURE WITH EMERGENCY BATTERY PACK. NL INDICATES FIXTURE ON NIGHT LIGHT CIRCUIT.
	RECESSED OR SURFACED MOUNTED DOWN LIGHTING FIXTURE.
	WALL MOUNTED H.I.D. WALL PACK LIGHTING FIXTURE.
	ILLUMINATED EXIT SIGN WITH NUMBER OF FACES AND DIRECTION OF EGRESS ARROWS AS INDICATED.
	STANDARD 20A, 120/277V-1~ SPST TOGGLE SWITCH MOUNTED AT +48" A.F.F.-U.O.N. a,b - DENOTES TWO SWITCHES AND THEIR RESPECTIVE CONTROL IDENTIFICATION.
	WALL MOUNTED LIGHT SENSOR WITH ADJUSTABLE TIME DELAY OFF SETTING.
	STANDARD 20A, 120V-1~ GROUNDING TYPE DUPLEX RECEPTACLE MOUNTED AT +15" A.F.F. - U.O.N.
	SPECIAL RECEPTACLE MOUNTED AT +15" A.F.F. - U.O.N.. AMPS - VOLTS & PHASE AS INDICATED ON PLANS. VERIFY NEMA CONFIGURATION WITH INSTALLED EQUIPMENT SUPPLIER PRIOR TO PLACING ORDER.
	STANDARD 20A, 120V-1~ ISOLATED GROUND TYPE FOURPLEX RECEPTACLE MOUNTED AT +15" A.F.F. - U.O.N.
	115 VOLT QUADRUPLX DEDICATED POWER FOR P.O.S.
	208 VOLT SINGLE CONV OUTLET
	VAPOR PROOF LIGHT FIXTURE
	FLEX CONNECTION
	HEATING ELEMENT
	FUSED DISCONNECT SWITCH. AMPS - VOLTS & POLES AS INDICATED ON PLANS.
	MANUAL MOTOR STARTER SWITCH. RATED @ 1H.P. MIN-120V - 1~ -U.O.N. PROVIDE COMPLETE WITH THERMAL OVERLOAD PROTECTION.
	JUNCTION BOX. SIZED BY THIS CONTRACTOR PER ACTUAL NUMBER OF CONDUITS AND / OR CONDUCTORS PASSING THRU.
	MOTOR OUTLET. H.P. OR F.L.A. - VOLTS & PHASE AS INDICATED. VERIFY ELECTRICAL CHARACTERISTICS AND CONNECTION REQUIREMENTS WITH INSTALLED EQUIPMENT MFR. PRIOR TO ANY ROUGH-IN WORK.
	BRANCH CIRCUIT PANELBOARD. FLUSH OR SURFACE MTD. AS INDICATED.
	TELEPHONE PLYWOOD BACKBOARD.
	THERMOSTAT FURNISHED AND INSTALLED BY MECHANICAL CONTRACTOR. PROVIDE J-BOX AT +72" A.F.F. - U.O.N.. PROVIDE 1/2" C.O. TO ASSOCIATED HVAC UNIT.
	TELEPHONE OUTLET MOUNTED AT +15" A.F.F. - U.O.N.. PROVIDE 3/4" C.O. TO MAIN TELEPHONE BACKBOARD OR AS INDICATED ON PLAN.
	CONDUIT CONCEALED WITHIN BUILDING WALLS OR CEILING SPACE. TICK MARKS INDICATE QUANTITY OF #12 THHN / THWN CONDUCTORS - U.O.N.. CONDUIT SHOWN WITH NO TICK MARKS INDICATE 2 #12 THHN / THWN CONDUCTORS - U.O.N.. CONDUIT SHALL BE 3/4" MINIMUM - U.O.N..
	CONDUIT ROUTED BELOW FINISHED GRADE AND / OR CONCRETE SLAB. TICK MARKS INDICATE QUANTITY OF #12 THHN / THWN CONDUCTORS - U.O.N.. CONDUITS SHOWN WITH NO TICK MARKS INDICATE 2 #12 THHN / THWN CONDUCTORS - U.O.N.. CONDUIT SHALL BE 3/4" MINIMUM - U.O.N.. INCLUDE CODE SIZED COPPER BOND CONDUCTOR (NOT SHOWN ON PLAN) IN ALL NON-METALLIC CONDUIT RUNS.
	CIRCUIT CONDUCTORS WITH ISOLATED GROUND.
	HOMERUN TO DESTINATION AS INDICATED. REFER TO CONDUIT SYMBOL ABOVE.
	INDICATES CONDUIT DROP WITHIN BUILDING WALL. REFER TO CONDUIT SYMBOL ABOVE.
	INDICATES CONDUIT RISER WITHIN BUILDING WALL. REFER TO CONDUIT SYMBOL ABOVE.
	FIRE ALARM DUCT SMOKE DETECTOR
	REMOTE PULL STATION FOR ANSUL SYSTEM.
	LIGHTING FIXTURE DESIGNATION
	DETAIL REFERENCE
	KEYNOTE REFERENCE
	KITCHEN EQUIPMENT DESIGNATION
	FIRE ALARM DUCT SMOKE DETECTOR
	REMOTE PULL STATION FOR ANSUL SYSTEM.
	LIGHTING FIXTURE DESIGNATION
	DETAIL REFERENCE
	KEYNOTE REFERENCE
	KITCHEN EQUIPMENT DESIGNATION

# ELECTRICAL-UTILITY LEGEND

WHERE APPLICABLE			
	SINGLE POLE		SURFACE MOUNTED FIXTURE
	THREE WAY		PENDANT / HANGING FIXTURE
	FOUR WAY		WALL SCONCE (VERIFY HEIGHT & TYPE W/ ARCHITECT OR OWNER)
	DIMMER		WALL MOUNTED LIGHT STRIP
	CHRISTMAS SWITCH		WALL MOUNTED (VERIFY HEIGHT & TYPE W/ ARCHITECT OR OWNER)
	VACANCY SENSOR		RECESSED FIXTURE
	DUPLEX OUTLET , AFCI		DIRECTIONAL/STEP LIGHTS
	GROUND FAULT CIRCUIT INTERRUPTER		RECESSED, DIRECTIONAL WALL WASHER
	HALF SWITCHED, AFCI		RECESSED, WATERPROOF GFCI PROTECTED, NON-METALLIC RIM, LIGHT FIXTURE
	DOUBLE DUPLEX, AFCI		FLUORESCENT LIGHT (TO BE 40 LUMENS PER WATT MIN.)
	WEATHER PROOF		WATERPROOF GFCI W/HUMIDISTAT PROTECTED, NON-METALLIC RIM FAN EXHAUST RATE = 50 CFM INTERMITTENT 20 CFM CONTINUOUS VENTED TO OUTSIDE
	220 VOLTS		FUEL GAS
	CABLE TV		CEILING FAN WITH LIGHTS: CEILING FANS WEIGHING OVER 35 LBS. CANNOT BE SUPPORTED BY AN ELECTRICAL BOX. THEY MUST BE SUPPORTED AS REQUIRED BY SEC 370-23 & 422-18. FAN AND LIGHT TO BE SWITCHED SEPARATELY
	TELEPHONE		LOOSE KEY VALVE
	ETHERNET		GARAGE DOOR OPENER CEILING MOUNT
	CHIMES		DISH WASHER
	PUSH BUTTON OR DOOR BELL		TRASH COMPACTOR
	GARAGE DOOR SAFETY SENSOR		GARBAGE DISPOSAL
	PHOTOELECTRIC CELL		WHOLE HOUSE VACUUM WALL INLET
	JUNCTION BOX		WHOLE HOUSE VACUUM TANK
	STUB FOR ICE MAKER		WATER FILTER
	ANTI-SYPHON HOSE BIB		
	CHANDELIER		
	LAMP POST		
	FLOOD /SPOT LIGHT		
	MOTION SENSOR		
	ELECTRIC PANEL OR SUBPANEL		
	ELECTRIC METER		



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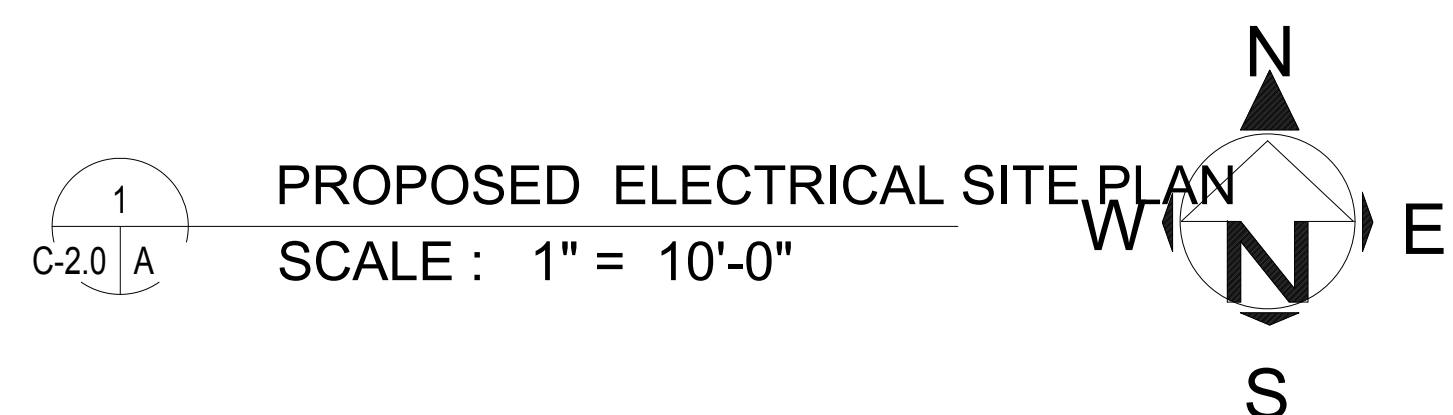
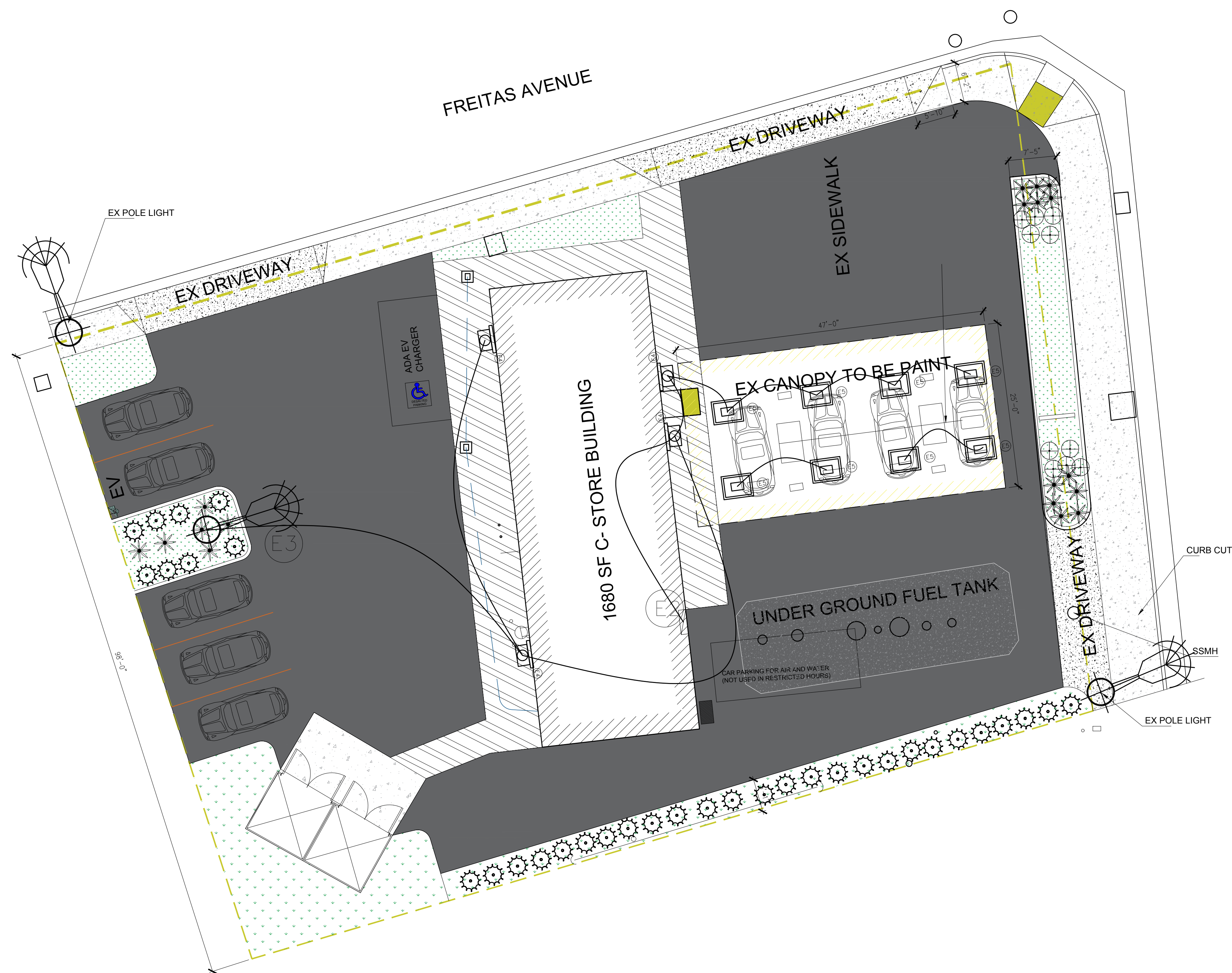
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 CHECKED:  
 APPROVED:

SHEET TITLE  
 GENERAL DETAILS

SHEET #  
 ED-1.0



## LEGEND USE IN DRAWING

- 1- ELECTRICAL POWER CABLE FOR LIGHTS (E1)
- 2- ELECTRICAL PANEL BOARD (E2)
- 3- POLE LIGHT FIXTURE (E3)



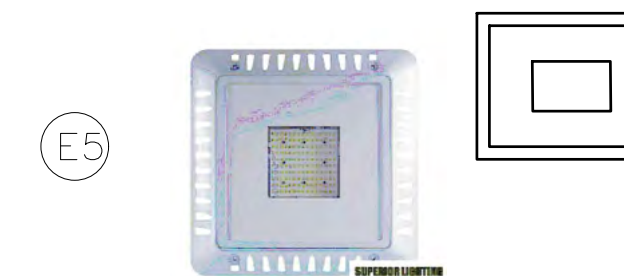
20,250 Lumens – 150 Watt – 3000 Kelvin – LED Parking Lot Fixture

- 4- WALL LIGHT FIXTURE (E4)

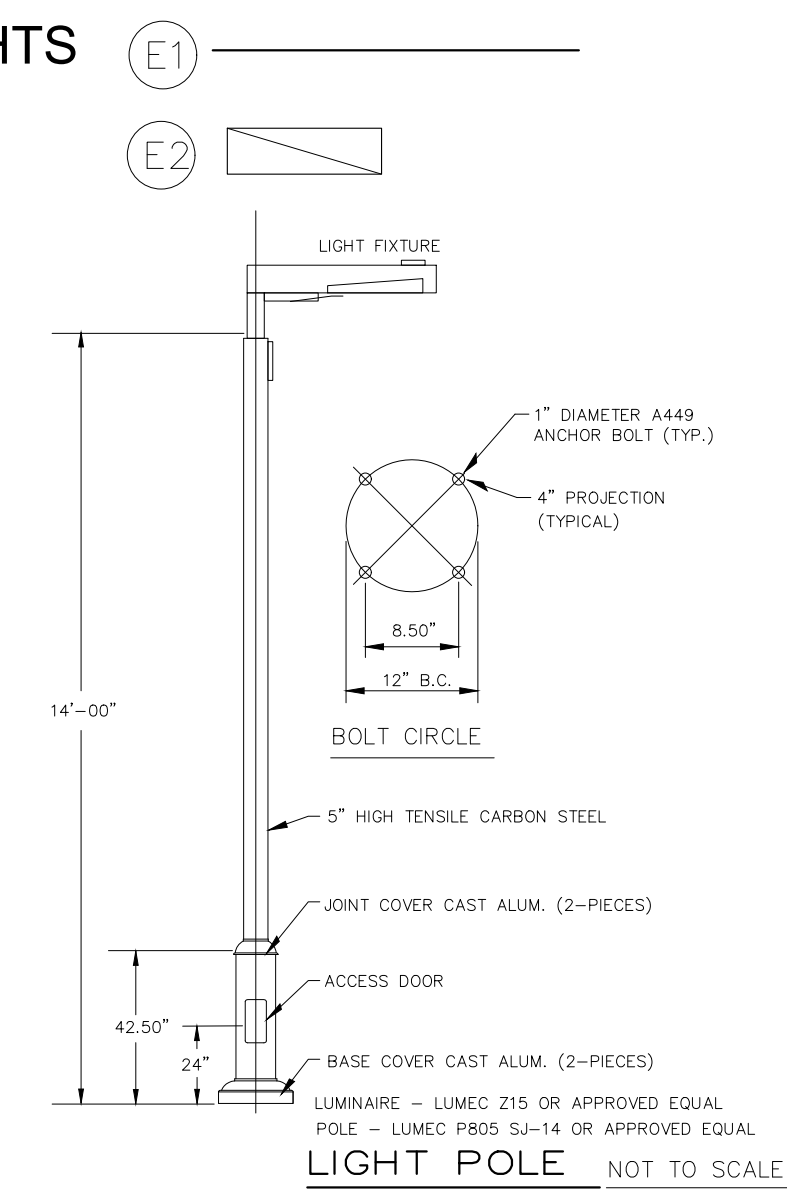


Dimension: 5.8in(L)x8.6in(H)x3.09in(D),  
Lumen :2500 Watt:15 W warm light  
3000K,IP65 waterproof.

- 5- CANOPY LIGHT FIXTURE (E5)



TECHNICAL SPECIFICATIONS:  
Voltage: 120-277V AC  
Wattage: 100 Watts  
Size: 15-3/4" x 15-3/4" x 4-1/2"  
Architectural UV resistant powder coat finish  
Beam: 120° Type V distribution  
Lumens: 10,000L  
IP65 Wet Location  
Color Temperature(CCT): 3,000K Daylight  
Operating Temp: -30°C to +50°C  
Operating Humidity: 20% to 90% RH



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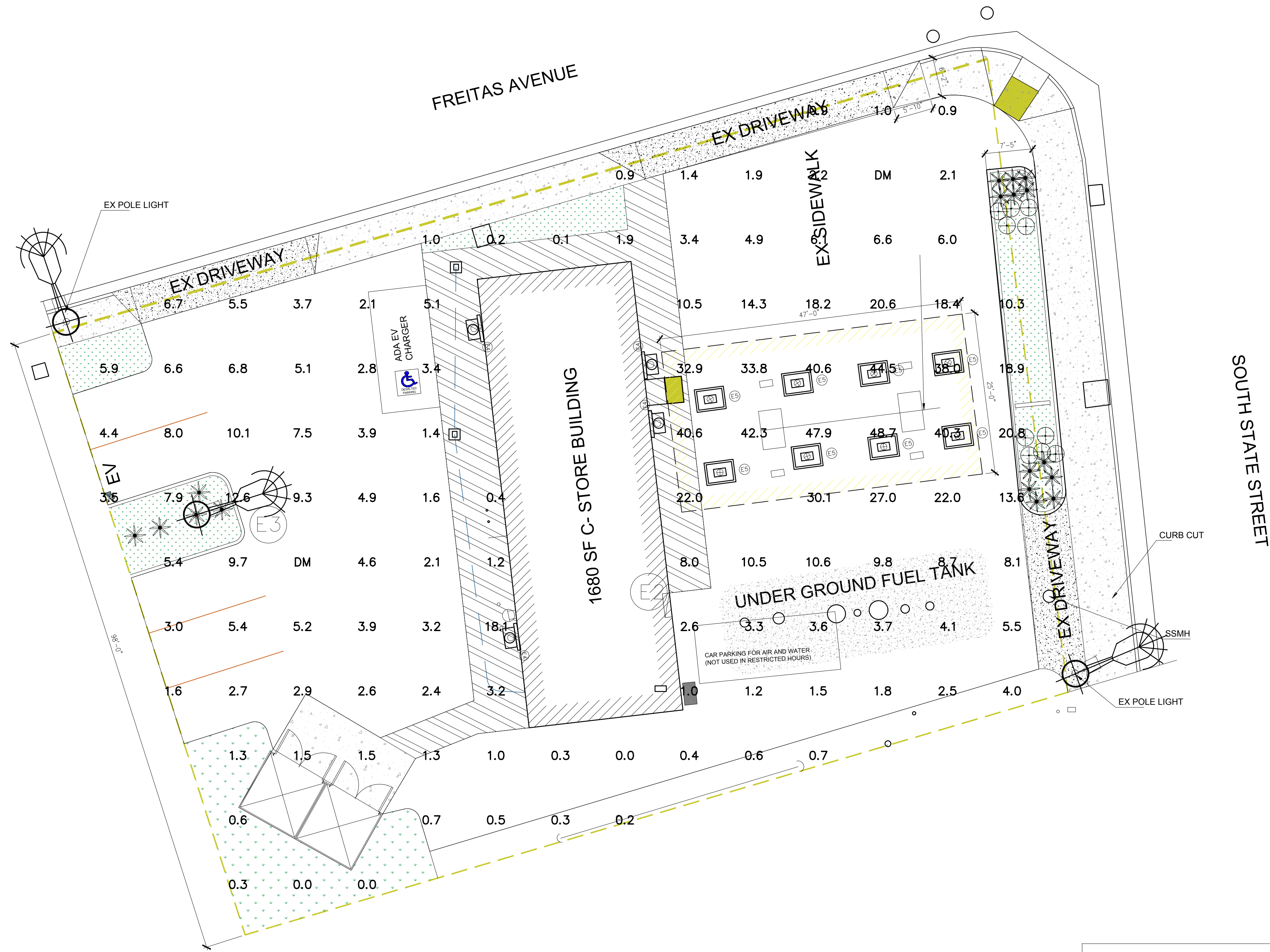
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SHEET TITLE  
PROPOSED ELECTRICAL SITE PLAN

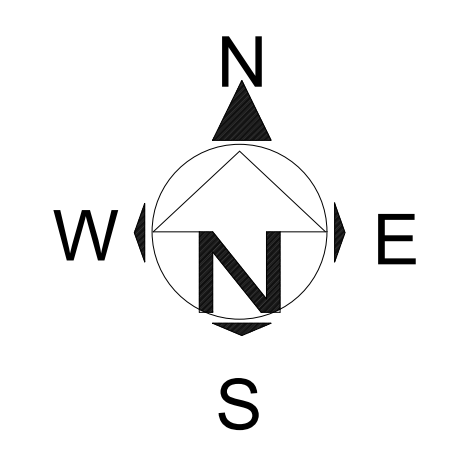
SHEET #  
**E-1.0**

# GENERAL PHOTOMETRIC SCHEDULE

AVERAGE FOOT-CANDLES	8.71
MAXIMUM FOOT-CANDLES	48.7
MINIMUM FOOT-CANDLES	0.0
MINIMUM TO MAXIMUM FC RATIO	0.00
MAXIMUM TO MINIMUM FC RATIO	41223.82
AVERAGE TO MINIMUM FC RATIO	7367.21



1 PHOTOMETRIC PLAN  
SCALE: 1" = 10'-0"



CALLOUT	SYMBOL	LAMP	DESCRIPTION	BALLAST	MOUNTING	MODEL	VOLTS	NOTE	NUMBER OF LIGHT	VOLTS	TOTAL LUMENS
canopy light	[Symbol]	(1) 100W	TG-CP380BS100W(4000K)	ELECTRONIC	CEILING	TG PHOTOELECTRIC TECHNOLOGY CO. LTD.	120V 1P 2W		8		11176
pole light	[Symbol]	(1)	LED	ELECTRONIC	POLE	MESTER LED LTD, MAL05150W27V50KHDSOT3	120V 1P 2W	120-277VAc 50/60Hz	1	120	21000
wall light	[Symbol]	(1) 20W 90 white LEDs, one W803-20W-15C38P2-2835-40 LED board	Surface mounted, cast aluminum housing, formed white enamel aluminum reflector, clear linear prismatic lens with frosted side sections.	ELECTRONIC	WALL	Topaz Lighting Corp, F-WPC/20W/CTS/B2-96 - 4000K Setting	120V 1P 2W	One HB-LPG020G-52 LED Driver	4	120	2500



PROJECT DIRECTOR:  
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M HAMAYOON  
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ARCHITECT OF RECORD:  
**BOULEVARD**  
Construction  
4080 TRUXEL ROAD, SUITE 100,  
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Call: +1(916) 330-4200  
FAX NO: 1(916) 914-2215  
Email: info@theboulevard.us

PROJECT:  
734 SOUTH STATE STREET UKIAH,  
CA 95482

REVISIONS:	DATE:	DESCRIPTION:	BY:

CURRENT ISSUE DATE:  
01,24,2024  
ISSUED FOR:  
01,24,2024  
CONTROL NO.:

DRAWN BY: BOULEVARD  
CHECKED:  
APPROVED:

SHEET TITLE  
PHOTOMETRIC PLAN

SHEET #  
**E-2.0**



**PROJECT REVIEW REFERRAL**  
**Please provide comments by:**

The City of Ukiah Community Development Department is soliciting input on the project described below for use in the staff analysis, CEQA document, and public hearing documents. Please identify any questions you have or missing information you need to complete your review, as well as suggested Conditions of Approval.

<b>TO:</b>	
City Planning Manager	Mendocino County Planning and Building
City Building Official	Mendocino County Surveyor/Assessor
City Public Works Dept.	Mendocino Transit Authority
Ukiah Valley Fire Authority	US Army Corps of Engineers
City Police Dept. Captain	Regional Water Quality Control Board
City Police Dept. Community Service Officer	CA Dept. of Fish and Wildlife
City Electric Utility Dept.	CA Dept. of Transportation
Ukiah Municipal Airport Operations Manager	Sonoma State Northwest Information Center
Airport Land Use Commission	AT & T
Mendocino County Air Quality	PG & E (gas)
Mendocino County Environmental Health	PG & E (Land Rights)
California Military Land Use Office	Other:
<b>FROM PROJECT PLANNER:</b>	

<b>PROJECT INFORMATION:</b>	
<b>Project Name &amp; Permit #:</b>	
<b>Site Address &amp; APN:</b>	
<b>General Plan:</b>	
<b>Zoning:</b>	
<b>Airport Compatibility Zone:</b>	
<b>Date Filed:</b>	
<b>Resubmittal:</b>	
<b>Date Referred:</b>	
<b>Prev. Projects on Site (include file #)</b>	
<b>Applicant/Agent Name:</b>	
<b>Phone:</b>	
<b>Email:</b>	
<b>Project Summary:</b>	

Please indicate whether you have comments and sign and date below. Return this Project Referral Form by the date noted above to the Project Planner listed.

**Name and Affiliation/Department (please print)** \_\_\_\_\_

- No Comment
- Comments / Conditions of Approval Attached

\_\_\_\_\_  
**Signature** **Date**

**Comments / Conditions of Approval:**

A large, empty rectangular box with a dotted border, intended for entering comments or conditions of approval.



February 15, 2024

Joshua Montemayor  
City of Ukiah

Re: File No. 24-9523  
734 S State St

Dear Joshua Montemayor,

Thank you for providing PG&E the opportunity to review the proposed plans File No. 24-9523 dated 2/13/2024. Our review indicates the proposed improvements do not appear to directly interfere with existing PG&E facilities or impact our easement rights.

Please note this is our preliminary review and PG&E reserves the right for additional future review as needed. This letter shall not in any way alter, modify, or terminate any provision of any existing easement rights. If there are subsequent modifications made to the design, we ask that you resubmit the plans to the email address listed below.

If the project requires PG&E gas or electrical service in the future, please continue to work with PG&E's Service Planning department: <https://www.pge.com/cco/>.

As a reminder, before any digging or excavation occurs, please contact Underground Service Alert (USA) by dialing 811 a minimum of 2 working days prior to commencing any work. This free and independent service will ensure that all existing underground utilities are identified and marked on-site.

If you have any questions regarding our response, please contact the PG&E Plan Review Team at [pgeplanreview@pge.com](mailto:pgeplanreview@pge.com).

Sincerely,

PG&E Plan Review Team  
Land Management

**CITY OF UKIAH**  
**DESIGN REVIEW BOARD REGULAR MEETING MINUTES**  
Held in person and via Zoom teleconference  
**April 25, 2024**  
**3:00 p.m.**

**1. CALL TO ORDER**

The meeting was held in person and virtually via Zoom teleconference. Chair Tom Liden called the Design Review Board meeting to order at approximately 3:00 p.m.

Chair Tom Liden presiding.

**2. ROLL CALL**

**Members Present:** Hawkes, Gordon, Meaux and Chair Liden. **Absent** Akin.

**Staff Present:** Joshua Montemayor, Contract Planner; Jesse Davis, Chief Planning Manager;

**3. CORRESPONDENCE**

None received.

**4. APPROVAL OF MINUTES**

**Motion/Second:** Gordon/Hawkes made a motion to approve the minutes of the March 28, 2024 DRB minutes.

Motion carried by the following roll call votes: AYES: Gordon, Liden, Hawkes. NOES: None. ABSENT: Akin.

**5. COMMENTS FROM AUDIENCE ON NON-AGENDA ITEMS**

No public comments on non-agenda items.

**6. NEW BUSINESS**

- 6a. Request for review and recommendation to the Zoning Administrator regarding a Minor Site Development Permit for refurbishment of an existing gas station, including conversion of the existing garage area for retail use, updates to the façade, improvements to the parking lot, installation of new landscaping, and revised signage at 734 South State Street; APN 003-031- 78; File No. 24-9253**

Chief Planning Manager J. Davis presented the item.

Public comment period was opened and closed, as no members of the public were in attendance in-person, nor online via teleconference. The Applicant did address comments and questions of the Design Review Board.

On a Motion from Member Gordon and a Second from Member Hawkes, the DRB unanimously voted to recommend the approval of the proposed project to the Planning Commission, with the following recommendation:

- 1) Recommendation to the Zoning Administrator to approve the project with the suggestion that electric vehicle charging, and alternative energy improvements be incorporated (e.g. solar panels), as feasible.

Motion carried by the following roll call votes: AYES: Gordon, Liden, Hawkes. NOES: None. ABSENT: Akin.

**6b. Request for review and recommendation to the Planning Commission regarding the construction of a single-story commercial shell structure featuring a drive-thru at 1294 N. State Street; APN 001-370-36; File No. 17-3069**

Contract Planner Joshua Montemayor presented the item.

Public comment period was opened and closed, as no members of the public were in attendance in-person, nor online via teleconference. The Applicant did address comments and questions of the Design Review Board.

On a Motion from Member Hawkes and a Second from Member Gordon, the DRB unanimously voted to recommend the approval of the proposed project to the Planning Commission, with the following recommendation:

- 1) Recommendation to the Planning Commission to approve the project, including the drive-thru with the suggestion that (1) any associated traffic study for the proposed drive-thru shell structure be deferred until a specific tenant or use has been identified; (2) provide more effective pedestrian access (e.g. stepping stones) through proposed perimeter landscaping, and; (3) due to the overhang of the existing structure and identified circulation constraints, reduce the number of trees directly in-front of the existing and proposed commercial units by allowing for a reduction in the required percentage of shade coverage for paved areas.

Motion carried by the following roll call votes: AYES: Gordon, Liden, Hawkes. NOES: None. ABSENT: Akin.

**7. MATTERS FROM THE BOARD**

None

**8. MATTERS FROM STAFF**

None

**9. ADJOURNMENT**

There being no further business, the meeting adjourned at approximately 3:59 p.m.



**TO:** Design Review Board

**FROM:** Joshua Montemayor, Contract Planner

**DATE:** April 24, 2024

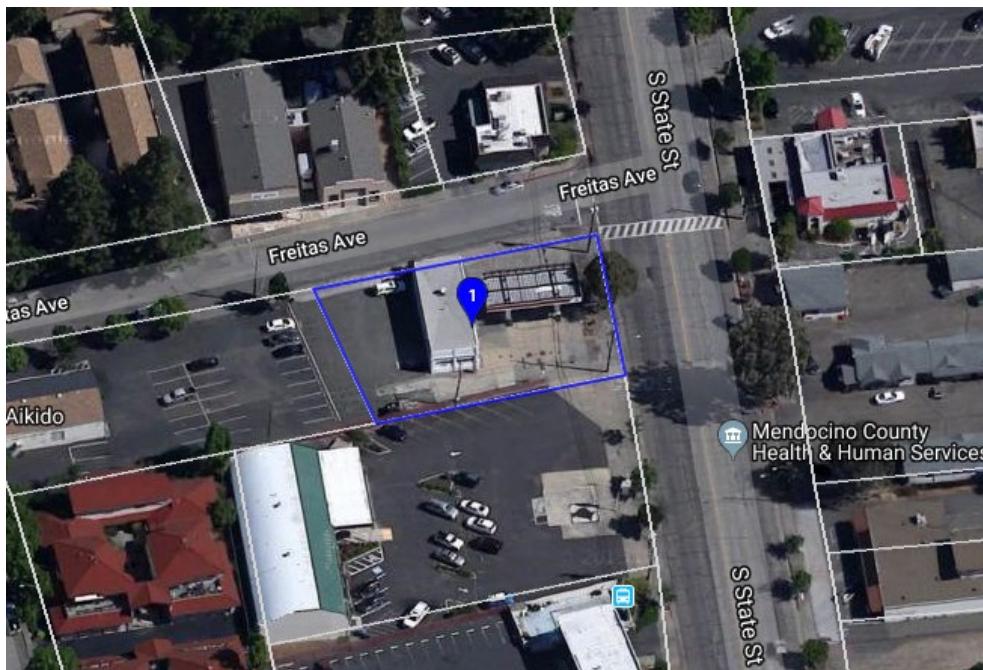
**SUBJECT:** Request for review and recommendation to the Zoning Administrator to allow a Minor Site Development Permit for the refurbishment of an existing gas station, focusing on updating the commercial building's façade, improvements to the parking lot, installing new landscaping, and other site-related enhancements at 734 South State Street; APN 003-031-78; File No. 25-9523.

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## LOCATION & EXISTING CONDITIONS

The ±0.32-acre project site (APN 003-031-78) occupies the corner of South State Street and Freitas Avenue, accessible from both thoroughfares. Currently, the site accommodates a 1,680-square-foot commercial building, formerly utilized for auto detailing services. In addition, the site features inactive gas pumps predating 2019 and an underground fuel tank. Parking, consisting of 11 spaces with faded markings, is situated at the rear of the property. Minimal landscaping is present, with only three trees adorning the frontage—two along Freitas Avenue and one facing South State Street. A chain-link fence with vinyl slats delineates the southern boundary, separating it from an adjacent parking lot. Furthermore, the rear of the building opens toward another neighboring parking area to the west. Figure 1 provides a location map of the project. Photos of the existing site are provided in Figure 2.

**Figure 1 – Location Map**



**Figure 2 – Existing Site Photos**  
*View via South State Street*



*View via Freitas Avenue, front*



*View via Freitas Avenue, rear*





## **BACKGROUND**

In April 1993, Use Permit Application 93-10 was granted, allowing a full-service gas station with a new underground fuel tank, and the implementation of a new sign program at 734 South State Street. The building itself, constructed before 1961, previously housed Redwood Tree Service, offering gas station and auto repair services until its closure several years ago.

In 2019, the property underwent partial reactivation through the approval of a Minor Use Permit (File No. 19-4525) for operating a car detailing and window tinting business within a section of the commercial building. However, the gas pumps remained inoperative. Since 1993, the gas station pumps and underground fuel tanks have remained non-operational and unchanged.

Presently, the property lacks a valid permit to operate the underground storage fuel tank, as the previous permit expired in December 2023. Therefore, the property owner will need to renew the permit to operate, which is issued by the Mendocino County Environmental Health Department.

## **PROJECT DESCRIPTION**

The project aims to reactivate the gas station and fuel pumps while also undertaking refurbishments to the existing building and parking lot. Comprehensive design details provided by the applicant can be found in Attachment 1. Below is a summary of the proposed improvements:

- Remodeling the building façade involves closing two existing roll-up doors and one swing door, replacing them with windows, and removing windows at the rear of the building. The top of the existing roof will be extended with a new parapet, with an overall building height of 18 feet to the top of the parapet.
- Reactivating the existing fuel pumps and repainting the existing gas station canopy to match the building's aesthetics.
- Enhancing the existing monument sign, which will remain in its current location, and installing new building signage featuring the business name "Express."
- Parking lot improvements will include new striping, upgraded lighting, and the installation of EV charging stations.
- Approximately 1,740 square feet, equivalent to 12.5% of the total site area, will be dedicated to new landscaping. This will involve the creation of new tree wells within the parking lot, planting six additional trees, infilling landscape planters with accent shrubs, and installing an evergreen hedge along the southern perimeter.
- Construct a new trash enclosure at the southwest corner of the property (currently no trash enclosure exists on site).
- Renovate the interior of the current commercial building to accommodate a mini-mart, with no proposed expansion to the existing footprint.



**COMMUNITY COMMERCIAL (C-1) ZONING CODE – DESIGN REQUIREMENTS**

The subject parcel is within the Community Commercial (C-1) zoning district. The purpose of the C-1 zoning district is to provide a broad range of commercial land use opportunities along the primary transportation corridors within the City. It is intended to promote and provide flexibility for commercial development and encourage the establishment of community-wide commercial-serving land uses.

Auto service (gas) stations are allowed uses in the C-1 Zoning District with a Use Permit. The property retains an existing Use Permit from 1993 (UP 93-10) to allow the operation of the gas station at 734 South State Street.

There are no proposed changes to the building footprint or any expansion to the gas pumps. All remaining features will remain the same and will be refurbished with a new façade as shown on the project plans. A summary of the relevant Design Requirements and compatibility with the proposed project is listed below.

<b>C-1 Zoning Design Requirements</b>	<b>Proposed Project</b>
Building Height – Max 50 ft.	Max 18 ft. to the top of the parapet.
Building Site Area – Existing	No change.
Setbacks – Existing	No change.
Parking – Existing	10 standard parking stalls 1 ADA parking stall 4 fuel pump parking
Landscaping - See separate section.	See Preliminary Landscape Plan, Sheet L 1-0.

**LANDSCAPING DESIGN GUIDELINES**

Under Ukiah Municipal Code (UKC) Sec. 9087.D, all development projects that require discretionary review shall include a proposed landscaping plan commensurate with the size and scale of the proposed development project. The applicant has provided a Preliminary Landscape Plan (Sheet L 1-0). The landscaping standards applicable to this project are included below for the Design Review Board’s consideration and assessment of the project:

- a) Landscape plantings shall be those that grow well in Ukiah’s climate without extensive irrigation. Native species are strongly encouraged.



- b) Deciduous trees shall constitute fifty-one percent (51%) of the trees proposed along the south and west building exposures; non-deciduous street species shall be restricted to areas that do not inhibit solar access.
- c) Parking lots with twelve (12) or more parking stalls shall have a tree placed between every four (4) parking stalls within a continuous linear planting strip rather than individual planting wells, unless clearly infeasible. Parking lot trees shall primarily be deciduous species, and shall be designed to provide a tree canopy coverage of fifty percent (50%) over all paved areas within fifteen (15) years of planting. Based upon the design of the parking lot, a reduced number of trees may be approved through the discretionary review process.
- d) Parking lots shall have a perimeter planting strip with both trees and shrubs.
- e) Parking lots with twelve (12) or more parking stalls shall have defined pedestrian sidewalks or marked pedestrian facilities of no less than three feet (3') in width within landscaped areas and/or separated from automobile travel lanes. Based upon the design of the parking lot, and the use that it is serving, relief from this requirement may be approved through the discretionary review process.
- f) Street trees may be placed on the property proposed for development instead of within the public right-of-way if the location is approved by the City Engineer, based upon safety and maintenance factors.
- g) All new developments shall include a landscaping coverage of twenty percent (20%) of the gross area of the parcel, unless because of the small size of a parcel as determined by the Planning Director. A minimum of fifty percent (50%) of the landscaped area shall be dedicated to live plantings.
- h) Landscaping plans shall include an automatic irrigation system and lighting plan.
- i) All required landscaping for commercial development projects shall be maintained.
- j) All healthy existing mature trees on development project sites shall be preserved and incorporated into the proposed landscaping plan, if feasible.

### **COMMERCIAL DESIGN GUIDELINES**

In 2007, Staff was directed by the Planning Commission to prepare review checklists to aid in the review of projects for consistency with the adopted design guidelines. On June 27, 2007, the Planning Commission adopted a Project Review Checklist for commercial development outside of the Downtown Design District in Table 1, below.

The Design Guidelines for Projects Outside of the Downtown Design District applies to this project and are included below for the Design Review Board's consideration and assessment of the project:



Table 1 - Commercial Projects Outside Downtown Design District			
Yes	No	N/A	
X			<b>Site Features:</b> Site design is compatible with the natural environment, and incorporates the major existing features ( <i>trees, landscaping, city creeks, riparian habitat, lot shape, size, relationship to surrounding area</i> ).
X			<b>Coordination:</b> Facilities are shared and coordinated with adjacent properties.
X			<b>Coordination:</b> Setbacks are compatible with character of adjacent frontages.
X			<b>Coordination:</b> Setbacks are minimized to enhance the pedestrian environment.
X			<b>Pedestrian Access:</b> Site has pedestrian orientation, consistent with uses, design, and architecture.
X			<b>Pedestrian Access:</b> Pedestrian elements are attractive and functional ( <i>walkways link parking to building entrances and other walkways; planters, street furniture, outdoor seating, pedestrian oriented signs, low level lighting provided</i> ).
X			<b>Pedestrian access:</b> Parking areas with 12 or more stalls: defined sidewalk or marked pedestrian facilities in landscaped areas or separated from traffic lanes required.
X			<b>Parking lots:</b> Decrease visual prominence and reduce heat island effect ( <i>locate behind buildings, divide into smaller lots, avoid large unbroken expanses of paving; emphasize screening, shading, landscaping</i> ).
X			<b>Landscaping:</b> Scale and nature of landscape materials is appropriate to the site and structures.
X			<b>Landscaping:</b> 20% of gross lot area landscaped / 50% live plantings; landscape redevelopment or reuse projects to extent feasible ( <i>Plants are of type, spacing and sizing to reach maturity within reasonable time. Hardy, drought tolerant, low maintenance species adapted to Ukiah climate are emphasized, parking lots trees also withstand heat, pollutants. Deciduous trees used on south and west. Automatic irrigation required for new commercial development</i>
X			<b>Landscaping:</b> Parking areas with 12 or more stalls: 1 tree per 4 stalls within continuous linear strips. Perimeter planting strips use trees and shrubs. Focus on deciduous trees achieving 50% shading within 10 years
X			<b>Landscaping:</b> Parking lots generally: Perimeter planting strips, Street trees selected from Ukiah Master Tree List required
X			<b>Signs:</b> Signs are compatible with architectural character of buildings ( <i>signage does not dominate site, uses compatible colors and material, lighting is restrained and harmonious, sandwich boards are creative/subdued co/or/minimal copy</i> ).



X			<b>Lighting:</b> Lighting harmonizes with site, building design, architecture and landscaping ( <i>lighting form, function, character, fixture styles, design and placement; lighting does not interfere with pedestrian movement</i> ).
X			<b>Energy Conservation:</b> Active and passive solar and other renewable energy design and devices are used ( <i>building orientation, landscaping, lighting, heating and cooling, photovoltaic system-ready or installed</i> ).
X			<b>Energy Conservation:</b> Devices are unobtrusive and complement design ( <i>solar panels flush with roof</i> ).
X			<b>Visual Appearance:</b> Buildings are visually cohesive, compatible and complementary ( <i>scale, proportion, design, style, heights, mass, setbacks</i> ).
X			<b>Visual Appearance:</b> Buildings exhibit variety and distinctiveness ( <i>but avoid overly obtrusive or overly monotonous designs, or strong contrast with adjacent buildings, creative use of natural and recycled materials; metal discouraged unless creative and consistent with Guidelines</i> ).
X			<b>Visual Appearance:</b> Variety of architectural features encouraged tied to comprehensive design theme ( <i>arches, raised parapets, cornices, eaves, windows, balconies, entry insets, roof angles and pitches, wall relief features</i> ).
X			<b>Visual Appearance:</b> Building exteriors compatible with surrounding properties ( <i>compatible materials, colors, quality, coordinated but not the same as surrounding properties, avoid strong or vivid colors unless they fit within local context, concrete block/exposed concrete on visible waifs finished in aesthetic manner</i> ).
X			<b>Visual Appearance:</b> Visible fences compatible with project and visually attractive ( <i>compatible colors, materials, styles; wire fences, high barriers and use for advertising discouraged</i> ).
X			<b>Visual Appearance:</b> Site and buildings are visually attractive from neighboring properties, traffic and corridors, and public spaces ( <i>service areas and devices screened, integrated and compatible with site features; common mailboxes architecture consistent and located close to building; above criteria is applied to areas visible to public view; rear and side views are visually interesting, coordinated and well-maintained</i> ).
X			<b>Maintenance:</b> Demonstrate consideration of site and building maintenance.
X			<b>Walkable and bikeable communities:</b> The project provides connections for walkers and bicyclists to the surrounding community ( <i>provides walking/biking facilities on the site, connects to nearly walking/biking facilities, provides shortcuts for walkers/bikers, project is located within ¼ or ½ of other places to walk</i> ).
X			<b>Walkable and bikeable Communities:</b> Sidewalks provide are convenient and safe access ( <i>sidewalks sufficiently wide, without</i>



			<i>obstruction; curbs, shade, lighting provided; buffers between walkers and traffic provided; safe and direct street crossings for walkers).</i>
X			<b>Walkable and bikeable communities:</b> Entrances provide convenient access ( <i>entrances adjacent to street, minimal setback, routes and accessways are well marked, sidewalks provided uninterrupted access to entrances, safe bike parking is located close to entrances</i> ).
X			<b>Green Building:</b> Sustainable site
X			<b>Green Building:</b> Water efficiency
X			<b>Green Building:</b> Energy
X			<b>Green Building:</b> Materials and resources
X			<b>Green Building:</b> Indoor environmental quality
X			<b>Visitability and Universal Design:</b> The site and its elements are accessible to people at differing stages, ages and circumstances: accessible primary and interior entrance and routes, accessible kitchen and bath space and devices, for dwellings accessible bedroom, common room, and devices).

**COMMERCIAL SIGNAGE**

Signage plans are included in the project proposal. The building façade is set to display the name "Express Mart" alongside the associated business logo. Additionally, the existing monument sign will undergo refacing to align with the aesthetics of the building and canopy, showcasing the name "Express." Moreover, the canopy will bear the name "Express" and its corresponding logo. All sign fonts and colors will match across each structure.

**MINOR SITE DEVELOPMENT PERMIT**

Projects exempt from the site development permit process include interior building remodels, repair and maintenance of structures or parking areas, and minor alterations on building exteriors. As this project proposes to refurbish an existing structure and subsequent site improvements to the parking area and accessory structures, it is considered a minor new development. Should the Design Review Board find that the site development permit application is minor, it shall be scheduled for a public hearing before the Zoning Administrator.

**STAFF REQUEST**

Staff is requesting the DRB review the project for consistency with the Community Commercial (C-1) Zoning Code, including the overall building design, proposed signage, and landscaping, and make a recommendation to the Zoning Administrator regarding the project.

**ATTACHMENTS**

1. Application Materials (Application, Site Plan, Elevations, and Landscape Plan)